

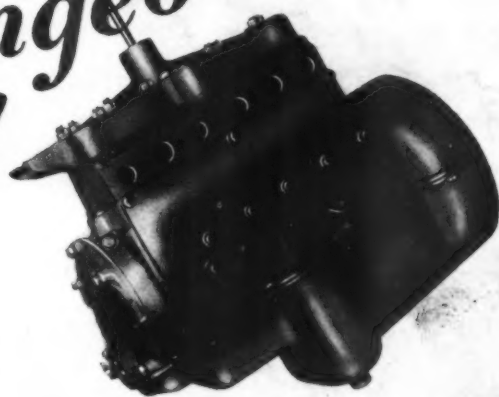
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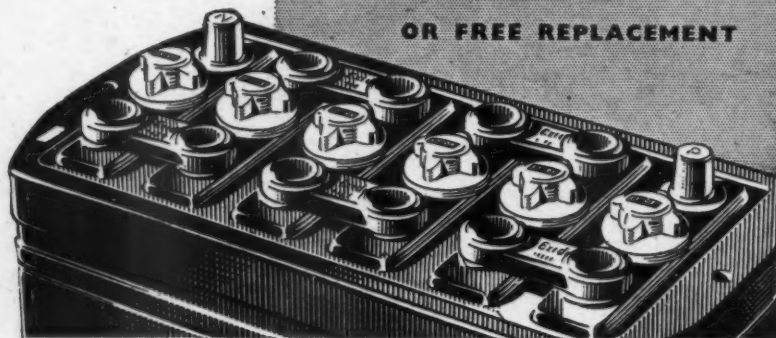
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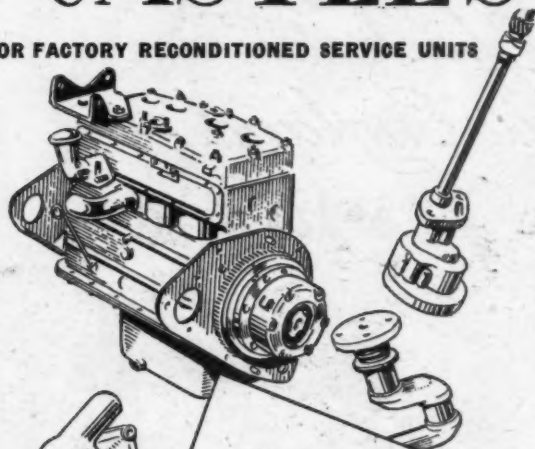
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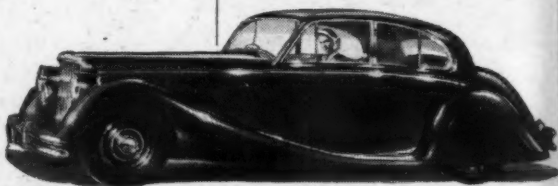


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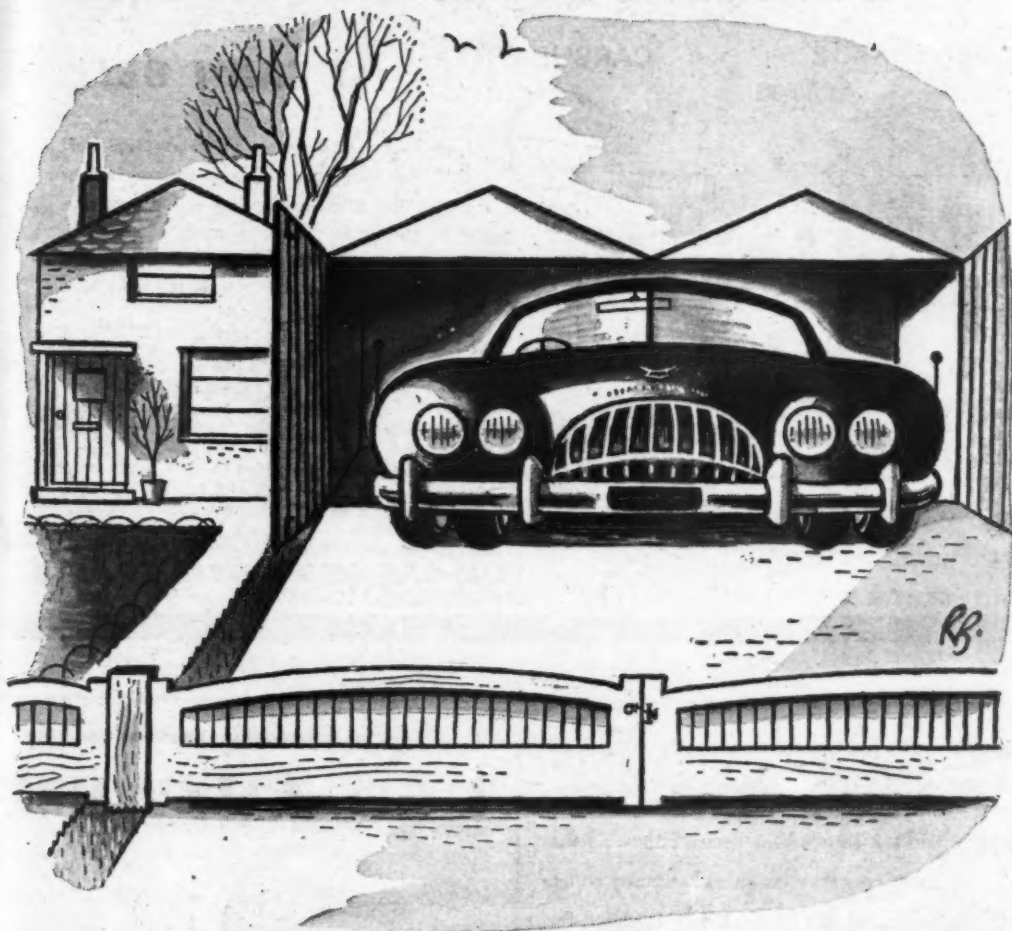
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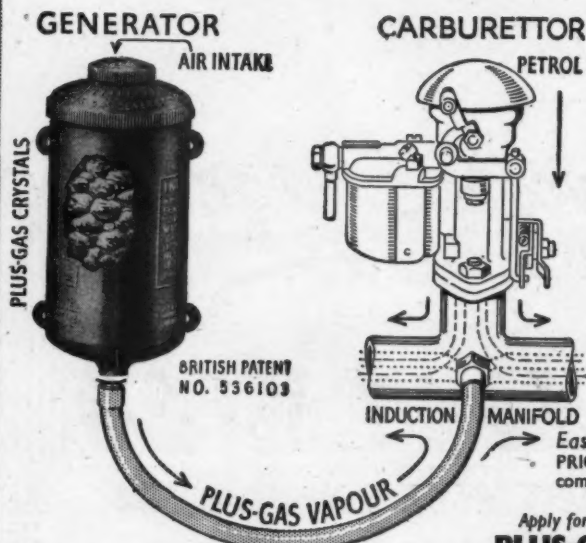


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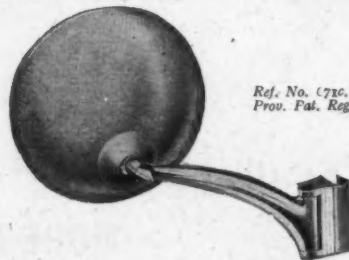


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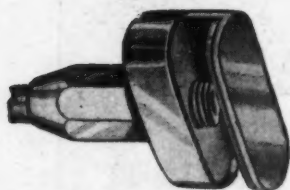
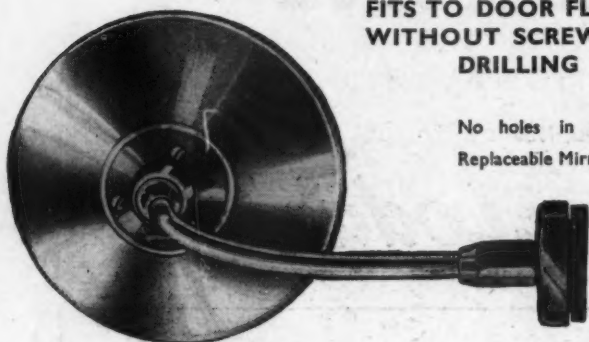
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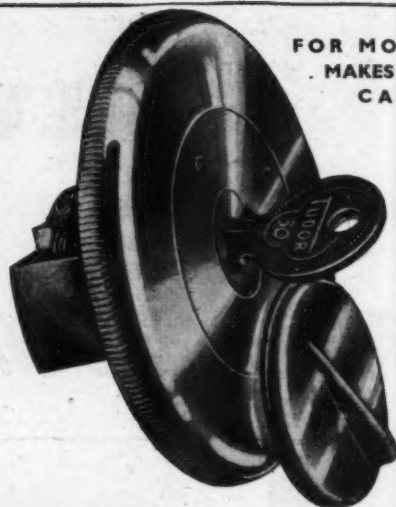


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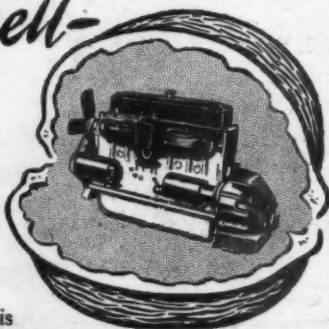
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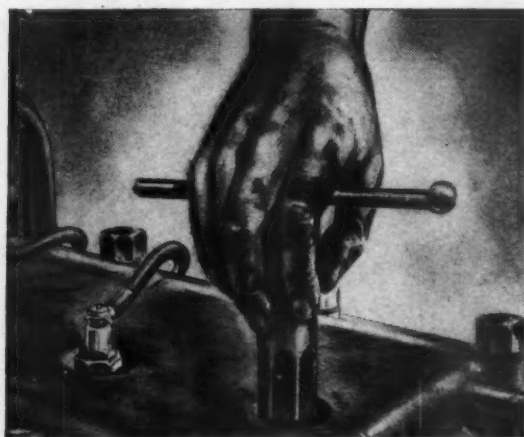
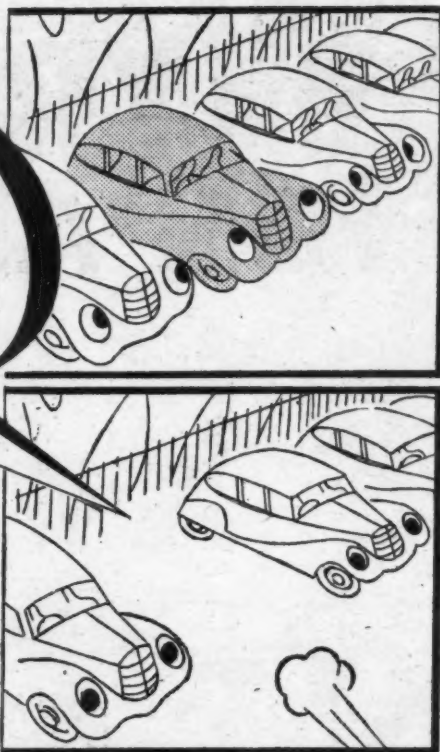
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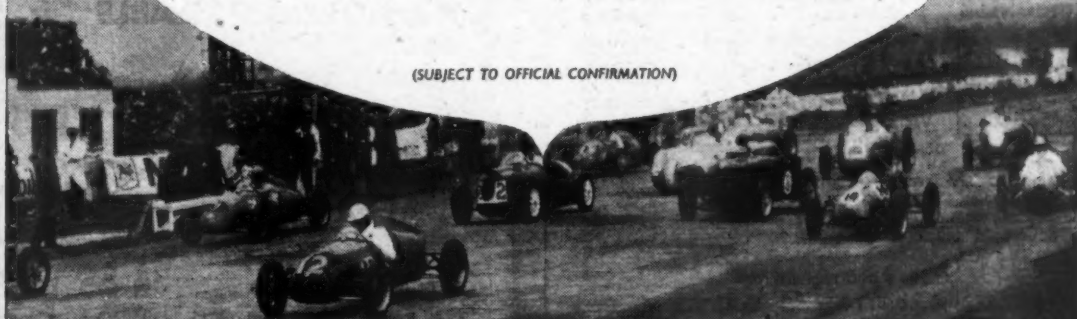
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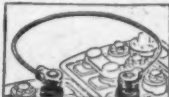
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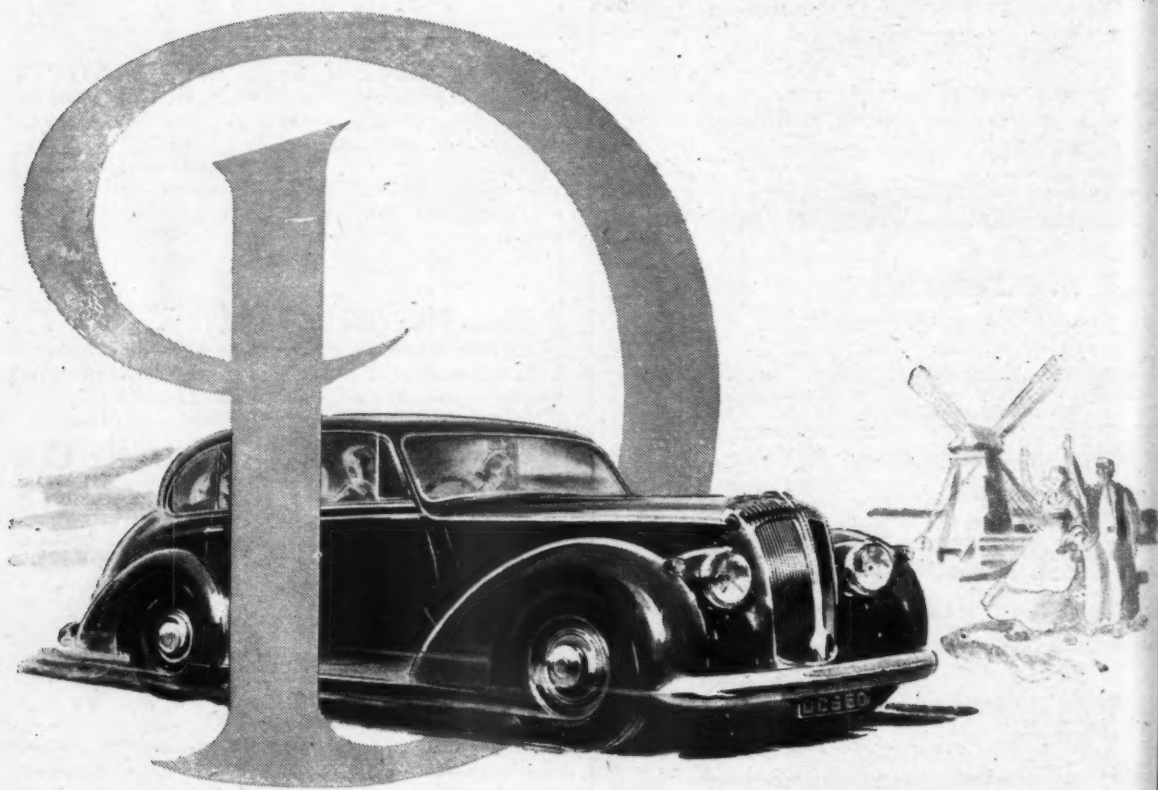
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The Autocar

FOUNDED 1895

No. 2863

FRIDAY, SEPTEMBER 22, 1950

Vol. XCV

Festival of Restriction

THE great traffic danger of the 1951 Festival of Britain lies, for motorists, not in the congestion concerning which Sir Harold Scott has uttered a grave warning, but in the fact that it presents the Government with an excellent opportunity to shut private cars out of Central London. This move has long been advocated by those to whom the bus queue represents the ideal of "equal shares," and the leading exponents are now coming out into the open by painting a rosy picture of fast journeys home by the workers unencumbered by the selfish motorist; they are discreetly silent about the possibility of there being no buses to catch owing to the perennial strike epidemic that afflicts London Transport.

The time to oppose this new restriction is now. If it is applied, with honeyed words about its temporary nature, just before the Festival opens on the South Bank of the Thames, the door will have been slammed on the car, for under the present administration temporary measures have an alarming habit of permanency. The organizations that exist for the protection of motorists should right away ask for an official assurance that no such measure is contemplated, and motorists should inform their M.P.s of their implacable opposition to any such restriction. Proprietors of service stations, hotels, theatres, restaurants and all such places should consider the loss of trade that would result.

The history of motoring is a battle history—against restriction. The car escaped its early bonds, and its popularity has given rise to others owing to the reluctance of authorities to cater for modern means of travel by providing adequate roads and parking space. It must not now be made the scapegoat for their dereliction of duty. Let the first battle of the preventive war commence.

Next Month's Show

ASSISTED once again by its compositors, *The Autocar* can look forward confidently to the forthcoming London Show, which opens on October 18—a month away. It is going to be a good show, and it is going to be once again a chiefly "export" show from the buying point of view, but that does not mean that the home motorist need come away inspired only by envy.

New models which make a clean breakaway from the previous style will not be many in number, but every make will show detail modifications based on road experience over a considerable time; the show will, therefore, display the British post-war car at its present ultimate—teething troubles long past, extensive road experience behind the type. As such, it is worth considerable study in order to discover the steps that have been taken, probably irrevocably, since 1939. Front independent suspension, with its pronounced effect on comfort, is an important one, as also is the general rise in engine size. Increased body space has been achieved, along with more room for luggage, and the driver has co-operated in obtaining frontal roominess by accepting bench-type seats and a steering column gear change.

Little change of note has taken place in engine design, apart from the size increase already mentioned, but it is interesting to observe the domination of overhead valves; there are very few side-valve engines left today, although design is now tending to point in the direction of valve schizophrenia, with overhead inlet and side exhaust; the resulting "F head" is, however, restricted to a few quality cars at the moment.

Will any of the post-war steps ultimately be retraced? We doubt it, believing that motoring is on the threshold of still longer strides into the future. The clutch pedal may become as vestigial as the appendix in the course of a year or two, and after that, as the turbocar demonstration earlier this year suggested, the normal gear box may follow it into the shadows of past evolution.

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TORRENTIAL T.T.

BRILLIANT JAGUAR VICTORY IN APPALLING CONDITIONS : ONLY FOUR RETIREMENTS

THE first post-war Tourist Trophy race, revived by the R.A.C. after a lapse of eleven years, was run over the new Dundrod circuit near Belfast last Saturday under the worst weather conditions imaginable. Rain poured down throughout the race, and a howling gale added to the difficulties and discomforts of drivers and spectators alike. Nevertheless, the event was closely fought and full of interest; victory went to brilliant young Stirling Moss, on the eve of his twenty-first birthday, driving in his first sports car race at the wheel of a Jaguar XK120. Second came Peter Whitehead in a similar car, followed by Bob Gerard in his Le Mans Replica Frazer-Nash.

Some criticism may be levelled at the organizers on the score that the race was not international in anything but name, every one of the thirty-one competing cars being British; but the reliability of the latter was ably demonstrated by the fact that in this gruelling three-hour event only four cars retired.



Stirling Moss, bedecked with laurels, receives the congratulations of Sir Basil Brooke after the race.

PRACTICE had indicated that the main struggle would lie between the *marques* Jaguar and Frazer-Nash, with Aston Martins close behind, making a strong bid for the team prize. Much depended on the weather, as dry roads had proved abrasive to the tyres, particularly of the fast and heavy Jaguars, while there was also speculation as to the possible necessity of refuelling stops. Obviously, a stop to change two wheels, using the normal jacking system of the car and—with the Jaguars—bolt-on wheels, might easily prove the decisive factor. Moss put up fastest practice lap at 81.39 m.p.h., representing 105.56 per cent of his set handicap speed, while Norman Culpan, in the 2-litre Frazer-Nash, achieved 77.61 m.p.h. (103.61 per cent). Two cars crashed during practice; Nick Haines, who was acting as reserve driver to Tony Rolt in the former's Jaguar, unfortunately hit a tree at Quarterlands Corners on his first lap during Friday's session, damaging the car beyond immediate repair but escap-

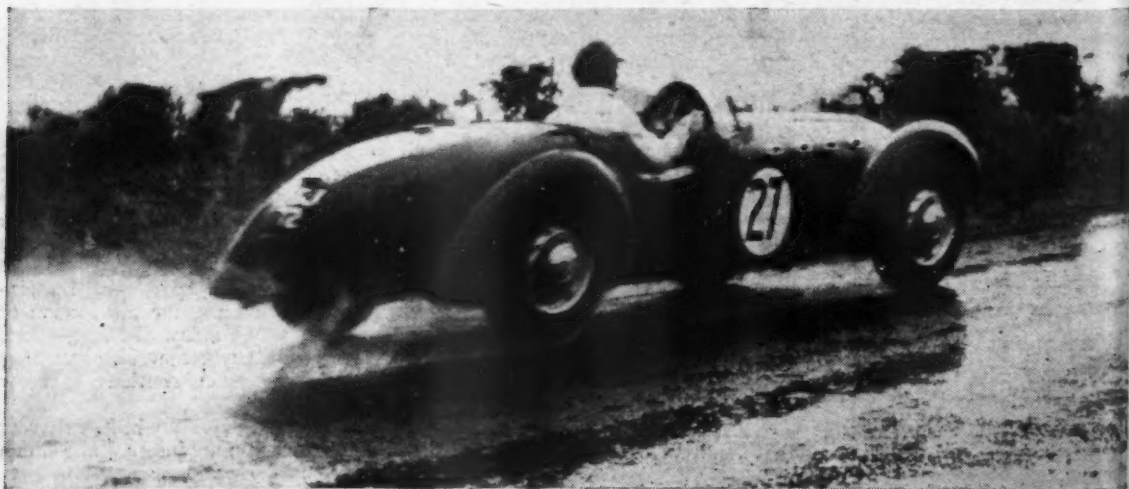
ing with minor cuts, while Jack Newton (of Notwen oil fame) was doubly unfortunate. Having crashed into the bank at Tornagrough on Thursday, damaging the front suspension, he rebuilt the Frazer-Nash with parts flown from England for the purpose, only to have the timing chain fracture during aerodrome tests on the Saturday morning, when no time remained for further work. Other non-starters were T. C. Harrison, whose Allard was not ready, and A. A. Baring, who decided that his new Frazer-Nash was insufficiently run in to give of its best.

Downpour

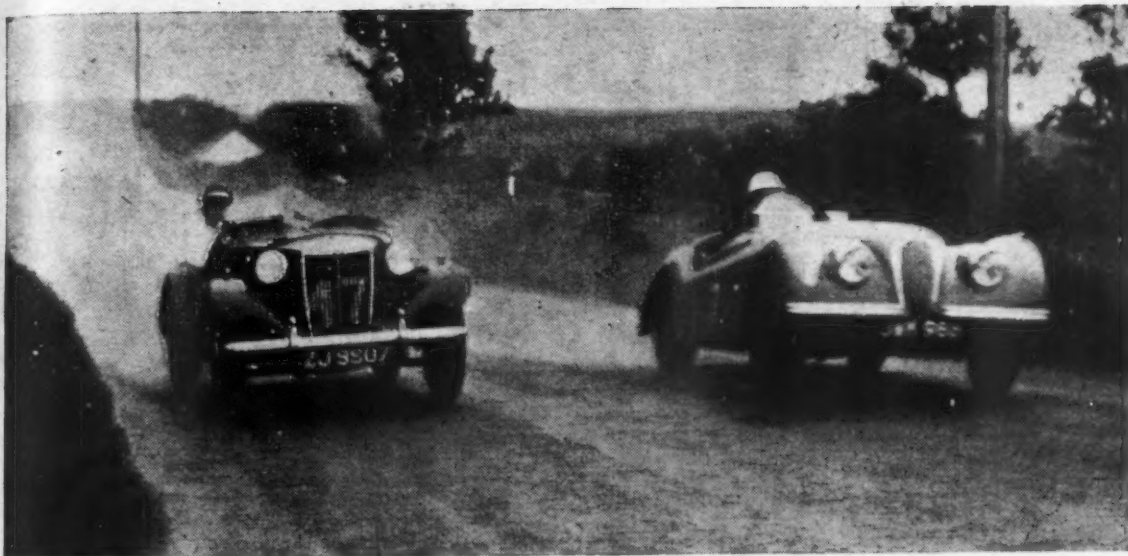
Race day dawned cold and blustery, with intermittent driving rain which, by lunchtime, had settled down to a steady downpour. These conditions suited the Jaguar team, who were thereby relieved of much of their anxiety concerning the durability of their tyres and brakes, but the unhappy pit staffs, officials, Press and spectators could see no good in the

situation at all. In the circumstances, there was a surprisingly large crowd for the start of the event, especially round the circuit at the various points of vantage, but very few hardy mortals braved the elements until the end. Car parks turned into bogs, cars in many cases being parked by the roadside in long queues miles from the course; programmes quickly became pulp, and picnic lunches were eaten under—to say the very least—conditions of great discomfort.

Eventually, just before two o'clock, the drivers lined up on the opposite side of the road from their cars; then Sir Basil Brooke dropped the flag and they splashed across and thankfully sought the comparative shelter of their cockpits. First car to come out of the line was Buncombe's H.R.G., but before he could reach the head of the line Leslie Johnson's white Jaguar had leapt into life and was away in the lead, followed by Stirling Moss, and Reg Parnell with the first Aston Martin. All the cars got



C. H. Masters' Silverstone Healey just about to enter the switchback section down to Cochranestown Corner.



The timed section of the straight was situated after the pit area; here Stirling Moss, travelling at 120 m.p.h., is seen overtaking the TD M.G. driven by "T. Flack."

away well; last—by a narrow margin—was the Jowett Jupiter driven by Tommy Wisdom.

A six-minute interval, and round came the leaders; Johnson and Moss, close together, with a very long lead from the pack of Aston Martins and Allards, with Whitehead's Jaguar sandwiched in the centre, and then the rest in a glorious bunch, half invisible for water, spray and mud. By the end of the third lap Moss was in the lead; Parnell headed the 3-litre class, Culpan the 2-litres, while Wisdom had recovered so well from his starting position that he was already in front of his M.G. and H.R.G. opponents in the smallest class of all. Wilson's Austin A.90 was out with gear box trouble, while Freed's Healey, which had been noticeably wild in practice, had hit a tree near Ireland's Corner, fortunately without injury to the driver.

Handicap Headaches

The race began to settle down, and much head-scratching could be seen in the pits as "Lofty" England, John Wyer, and the rest of the team managers, together with their tame mathematicians, commenced to wrestle with the problem of the formula and their position on handicap. Actually, Moss never lost the lead; but he was challenged in the initial stages of the race by Culpan's Frazer-Nash, while the remaining Jaguars and the Aston Martins were never very far away. Meanwhile some were in trouble; Wisdom parked the Jupiter in the escape road at Leathemstown, trudging back to the pits to report a supposed blown gasket, while Sydney Allard had got his car half into a field just after Wheeler's Corner. Retrieving it, he brought it to the pits to change a wheel, continuing undaunted thereafter. The three works TD M.G.s were now in line ahead, leading the 1½-litre class, but poor Peter Clark had the brakes seize on his H.R.G. at the Hairpin, coming slowly into the pits and retiring as a result.

On the fastest section of the course, between the pits and Leathemstown Corner, a timed section produced some interesting comparisons in the way of

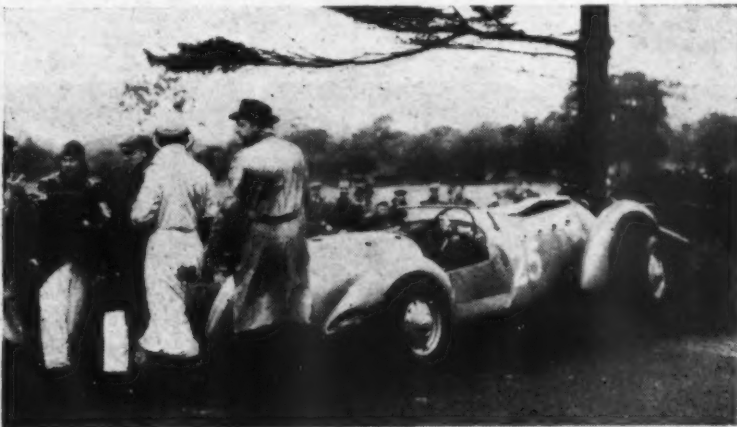
maximum speed. Two of the Jaguars were timed at just over 120 m.p.h., while Parnell's Aston Martin achieved 113, the Healey driven by Ulsterman E. J. Wilkinson, which had been going very well indeed, 108.1, and Bob Gerard's Frazer-Nash 106.8. The 1½-litres produced 83-85 m.p.h.; of course, there was a strong following wind at this point, which must be taken into account in considering all the above figures.

Order on Handicap at 3.30 p.m.: 1, Jaguar (Moss), 75.16 m.p.h., 97.49 handicap percentage; 2, Jaguar (Johnson), 73.45, 95.27; 3, Jaguar (Whitehead), 73.25, 95.01; 4, Frazer-Nash (Culpan), 71.02, 94.61; 5, Aston Martin (Parnell), 71.55, 94.39.

Parnell and Macklin had been circulating in nose-to-tail order with the first two Aston Martins, but Abecassis came up behind Reg when Macklin lost ground through taking to the escape road at Cochranestown Corner. Shortly after half-distance, those cars which could not carry sufficient fuel for the entire race were beginning to come in to the pits to refuel; first in was Warburton's Allard, the operation

taking three minutes, which the second Allard (driven by Ken Watkins) later reduced to two. The unfortunate Sydney Allard returned to the pits with trouble in the gear box, taking the opportunity to shelter in his van for a brief period while Tom Lush wrestled with the mechanism; however, little enough could be done, and Allard returned to the fray with only top gear in commission.

Culpan had been going very fast with the leading Frazer-Nash, but now slowed slightly, while Gerard (who had not previously been among the race leaders, although safely in second place in the 2-litre class) now increased speed considerably, catching and passing Culpan and setting off in pursuit of Macklin's Aston Martin; the latter was losing slightly to his team mates, possibly on account of brake troubles, as the car did not seem to have lost any speed. Conditions had, if anything, worsened—which nobody had thought possible—visibility at the Hairpin being down to a matter of 80 yards through low clouds.



Only car to be seriously damaged during the course of the race: W. Freed's Healey, after his crash at Ireland's Corner.

TORRENTIAL T.T.

—continued—

Still Moss continued, entirely unruffled and appearing unaffected by the weather; the other Jaguars were commencing to experience brake troubles, particularly Johnson's car, which locked one front wheel viciously at several points on the circuit. Whitehead crept up on him, little by little, and although Leslie put on a spurt and widened the gap once again, yet Peter once more came on relentlessly until, with little more than half an hour left to go, the red car appeared in front of the white, to occupy second place.

But on handicap Gerard was showing himself a force to be reckoned with; it now seemed certain that he would pass Johnson, and the Jaguar pit control



Drenched official waves the sodden flag to a damp Moss (camera was hydrated, too, at this stage).

made rapid calculations as to the possibility of his threatening Whitehead—or even Moss, should anything untoward occur. Had the race lasted longer, this might in fact have been so, but Bob could not quite make it, and as the chequered flag was raised to signal the end of the three hours, and the cars came down the road one by one, it was realized that the Jaguars had kept their positions to the end. Moss, in fact, had always had speed in hand, as he demonstrated on his last lap of all by setting up a new record lap at 77.61 m.p.h.—after three hours of concentrated motoring in appalling conditions!

Jaguars, of course, won their class, and also the team prize, by a narrow margin indeed from the consistent



N. R. Culpan's Frazer-Nash on Cochranestown.

Astons, which took the first three places in the 3-litre class. Frazer-Nashes did likewise among the 2-litres, while the TD M.G.s scored a resounding 1-2-3 success among the 1½s, although Lund (who had taken over the class lead when Dick Jacobs spun round) toured

slowly, having run a big-end in the last two laps. An excellent race in spite of everything; but it is to be hoped that next year's event will attract both some Continental opposition and—almost more important—some SUNSHINE!

RESULTS

			Mileage covered	Average speed	Handicap per centage
					m.p.h.
1.	Jaguar 3,442	(S. Moss)	225.452	75.15	97.47*
2.	Jaguar 3,442	(P. N. Whitehead)	222.392	74.13	94.15
3.	Frazer-Nash 1,971	(P. R. Gerard)	215.77	71.92	96.02*
4.	Aston Martin 2,580	(R. Parnell)	217.168	72.72	95.93*
5.	Aston Martin 2,580	(G. E. Abecassis)	217.148	72.38	95.49
6.	Frazer-Nash 1,971	(N. R. Culpan)	213.452	71.15	94.99
7.	Jaguar 3,442	(L. G. Johnson)	219.18	73.06	94.76
8.	Aston Martin 2,580	(L. Macklin)	215.354	71.78	94.60
9.	Healey 2,443	(E. J. Wilkinson)	211.791	70.59	93.37
10.	Frazer-Nash 1,071	(T. A. D. Crook)	207.739	69.25	92.46

11. Healey 2,443 (R. Richards), 91.10 per cent;
12. Healey 2,443 (C. H. Masters), 90.99; 13. Frazer-Nash 1,971 (D. H. Murray), 90.44; 14. Healey 2,443 (C. Mortimer), 87.93; 15. Healey 2,443 (R. M. Oliver), 87.87; 16. M.G. 1,250 (R. W. Jacobs), 87.54; 17. Allard 4,375 (G. Warburton), 85.31; 18. Allard 4,375 (R. Watkins), 84.73; 19. M.G. 1,250 (G. E. Phillips), 84.19; 20. M.G. 1,250 (E. W. K. Lund), 83.44; 21. H.R.G. 1,496 (J. Buncombe), 80.67; 22. M.G. 1,250 (J. J. Flynn), 79.86; 23. M.G. 1,250 (T. Flack), 78.73; 24. H.R.G. 1,496 (A. P. Hitchings), 78.64; 25. M.G. 1,250 (W. B. Groves), 77.34; 26. M.G. 1,250 (M. Henslett), 77.10; 27. Allard 4,375 (B. H. Allard), 69.16.

(Asterisk indicates class winner.)
Retirements: Austin A.90 2,660 (J. C. Wilson, 1 lap (gear box); Healey 2,443 (W. Freed), 2 laps (crash); H.R.G. 1,496 (P. Clark), 6 laps (brakes); Jowett Jupiter 1,486 (T. H. Wisdom), 5 laps (engine).

Team Prize (S.M.M.T. Trophy): 1. Jaguar (Moss, Johnson, Whitehead), 288.38 handicap percentage; 2. Aston Martin (Parnell, Macklin, Abecassis), 286.11; 3. Frazer-Nash (Gerard, Murray, Culpan), 281.45.
Greatest distance award: 1. Moss, 225.45 miles; 2. Whitehead, 222.392; 3. Johnson, 219.18.
Fastest lap: Moss, 5m 44s, 77.61 m.p.h.

Winners' Equipment

Jaguar (Moss): Esso fuel and oil. Lodge plugs, Dunlop tyres, Lucas ignition, Mintex brake linings, Newton and Girling shock absorbers, Lockheed brakes, S.V. carburetors.
Jaguar (Whitehead): Same as above, except Shell fuel and oil.
Frazer-Nash (Gerard): Shell fuel, Castrol oil, K.L.G. plugs, Dunlop tyres, Lucas and Delco-Remy ignition, Ferodo brake linings, Newton shock absorbers, Lockheed brakes, Solex carburetors.



Cochranestown Corner, with the Aston Martins of Parnell and Macklin.



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The Life Guards of Whitehall . . . proud, erect, they link us with the glory of Britain's past . . . a glory that we to-day are still upholding with the unrivalled skill that our forefathers have handed down to us . . . the skill, for example, that goes into the products of the Standard Motor Company, representing as they do in every detail of their design 'all that's best in Britain'.

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International Motor Show, Earl's Court
(Oct. 18th—28th)

STAND 145

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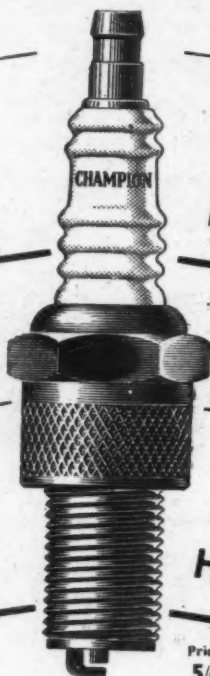
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NEWS and VIEWS

Oil Price Increase

THE price of Essolube motor oil has been increased by 6d a gallon. Various grades of grease are also very slightly affected. Essolube is made by the Anglo-American Oil Co., Ltd.

Kaiser-Frazer in Japan

AMERICAN Kaiser-Frazer cars are to be assembled in Japan under an agreement announced in Tokyo on September 8. Production will commence early in 1951 at a plant between Tokyo and Yokohama belonging to the Kawasaki Engineering company. The rate is likely to be between 500 and 1,000 a year, and the models concerned are the Kaiser and the Henry J.

Jupiter Increase

THE rise in costs of raw materials has caused an increase in the Jupiter range of the Jowett company. The 2-3-seater is now £850 plus £236 17s 3d British purchase tax, and the chassis (including instruments) is £525 plus £146 11s 8d tax.

Dollar Demand

THE demand for British cars in America and Canada has increased to a degree where orders now exceed supplies, said Sir William Welsh, North American representative of the Society of Motor Manufacturers and Traders, when he arrived at Southampton recently. Sir William said he would discuss the possibility of staging all-British motor shows similar to that at New York at other world centres.

July Production

NEW cars produced during July numbered 43,456, of which 34,435 were for export (about 80 per cent). Numbers made in the different categories were as follows, June figures (five-week month) being given for comparison:—

	July	June
Up to 1,600 c.c.	29,575	34,173
1,600 to 2,200 c.c.	8,113	8,777
Over 2,200 c.c.	5,763	7,078

Tour Des Logis

A PARTY representing the A.A., R.A.C., R.S.A.C. and the Cyclists' Touring Club is touring central France at the invitation of the French National Tourist Office. The purpose of the trip is to study the relatively little known part of the country and visit some of the small hotels called Les Logis de France, a new low-price development in touring which the French hope will attract more visitors.

Petrol Out of Hours

THE ever-present difficulty of obtaining petrol "out of hours" has been solved at Newport, Isle of Wight, by co-operation between the traders. Seven have taken space in the local newspaper, announcing that one will be open each week-end on a rota system. Previously petrol had been almost unobtainable between 5 p.m. on Saturdays and Monday morning.

Another approach to the problem has been made by a Nottingham radio

engineer, Mr. A. H. Throupe, who has devised a system of one-gallon containers locked up in compartments outside garages and in isolated areas. On this system motorists would buy a compartment key for the price of a gallon of petrol. When used, the key would be fixed in the lock, to prevent further use, until released by the proprietor.

Restyled Radiomobile

THIS year the Radiomobile model 100 is replaced by a new standard set, the 4100, which will be exhibited at the



Facia control unit of the new 4200 Radiomobile is very compact.

London Show. The price is down now to £15 15s, plus £3 18s 9d purchase tax. It is a long- and medium-wave five-valve superhet, with three push-buttons for tuning, and manual control. A de-

lux model, the 4200, is also being produced. This is an eight-valve superhet, with push-button and manual tuning, and four-position tone control. Price £21, plus £5 5s tax.

Technical changes in the new sets include the unit system of installation. The control unit, which is small for facia mounting, amplifier and speaker are separate in the 4100 and 4200, and this permits optimum placing of the speaker in the car. The control faces are very neat, with edge lighting, and serrated finger buttons in black.

To avoid company registration difficulties on the other side of the Atlantic, the firm has now changed its name to S. Smith and Sons (Radiomobile) Ltd. The company organization and ownership remain unchanged.

Export Expert Retires

THE retirement of Mr. B. J. Hegarty, A.S.S.A., A.C.I.S., from his post as a director of the Austin Motor Export Corporation was marked by an informal luncheon in Birmingham, on September 1. In wishing Mr. Hegarty good fortune in his retirement, Mr. George Harriman, who presided, said that export figures have grown from 250 vehicles per week sixteen years ago—when Mr. Hegarty joined Austin's as export manager—to 2,750 vehicles per week at the present time.

LIFEGUARDS ON PARADE

REPRESENTATIVES of various police forces were very interested in a demonstration which the Goodyear Tyre company arranged for them recently on an airfield near Wolverhampton. A car fitted with LifeGuards, the special inner tube designed to eliminate danger from tyre bursts, was equipped so that the right front tyre could be deflated at will by detonating electrically an explosive cap fitted to it. Another car, also equipped with LifeGuards, was then driven over iron spikes which caused both front and rear right tyres to have sudden "blowouts." After the demonstration some of the police representatives drove the cars themselves while the tyres were "burst," and in every case the drivers had no difficulty in keeping a straight course and pulling up smoothly.

The principle of the LifeGuard tube which, when described in *The Autocar* of September 2, 1949, was in production in two sizes, is that there is a tube within a tube. Thus the tube has an inner and an outer chamber, both being simultaneously inflated by a two-way valve. Normally the two-ply inner wall floats freely with the same pressure on both sides of it. Should the outer chamber suffer a sudden deflation by a blowout, the cover rides on the inner chamber until the air in it has slowly leaked away through the valve passage into the outer chamber and then through the puncture. Thus, sudden complete deflations are avoided and the driver has ample time to bring the car to rest safely under perfect control.

These tubes have to be built individually and the extra care and materials make them three or four times as

expensive as ordinary tubes. Against that, however, must be balanced the manufacturer's claim of longer life and greater safety. LifeGuards are now available in the following sizes: 5.25 x 16in, 5.50-5.90 x 16, 5.50 x 17, 5.75 x 16, 6.00-6.40 x 16, 6.25-6.50-6.70 x 16, and 6.50 x 17in.



The large hole made in the outer tube by the tests left the inner chamber unaffected.

Disconnected Jottings

BY THE SCRIBE
Drawings by Barry Appleby

Out of the Rut

AT a race meeting like the one at Silverstone on August 26 it is interesting to spend half an hour between events wandering round the car parks. A big car race brings together a remarkable collection of the older and half forgotten machines of the past and many examples of the ingenuity and interest of owners in substantially modifying their cars, as well as the hordes of everyday vehicles as like unto each other as the proverbial peas.

At the last Silverstone, during a return stroll to my car to pick up a raincoat, I was chiefly intrigued as I weaved in and out of the closely packed rows of cars in the "Green" enclosure by a vintage Rolls-Royce with the longest bonnet I think I have ever seen. It might have been nine feet long, there seeming to be much more of it than I recall being standard in the period to which the chassis belonged. The body was a two-door fixed-head coupé, not nearly as long itself as the bonnet. The finish was matt battleship grey and on the fascia was a silver plate engraved with, I imagine, the owner's name and the car's engine size—7.5 litres, stamping it as being one of the famous side-valve Silver Ghosts. A car with a history, no doubt.

Incidentally the possession of a car of distinctive appearance, or at least one of the less common colours, is a decided advantage in a park as densely populated as this. As I went back to the course for the next race I met an owner whom I had noticed earlier on a similar quest to mine, still searching for his car amidst the throng; and that with no press of people at that stage to complicate the hunt.

Nocturne

THE first time I noticed the almost incredible delicacy of the green of new leaves lit by street lamps from behind was after a long and bitter winter in Montreal. I was walking with a friend along the streets that lie at the base of the mountain from which the city takes its name and she said, "I would like a dress just that colour." The spring green was lit with a rare radiance, and ever since then I have taken particular note of the effect. It is rarely met, for most branches are cut away that obstruct the lighting of street lamps, but at the top of the hill to the west out of Egham, in Surrey, is a good example.

There is something particularly good about that stretch of road at night. Tree-hung, it has a blackness, except

at the one spot in question, that emphasizes the brilliance of head lamps. The curves are artistically gentle, and the glittering snake of cat's-eyes gives the unmistakable line of the road. A fast bend to the right, and shortly after you plunge down the long hill into Virginia Water; it is as well to note that there are 30 m.p.h. signs at the foot of the hill.

Climbing Power

DAILEDY Press reports (in a journal that should know better) of the tests of the British Jeep at Bagshot, Surrey, showed that vehicle to have some surprising hill-climbing powers. "Our Special Correspondent," writing from Bagshot, pointed out that "a test driver here today took the car up and down three test hills with gradients of 2, 3 and 4 in one." A Flying Officer sends me the cutting



Four in one.

with appropriate sketches as to how it might be done, and concludes with the wistful P.S., "Maybe the Russians will believe it—I hope so."

Personally I found the sub-title to the paragraph apt—"Use in Landings." It had need be if it is going to rocket up gradients of 4 in 1. After all, the Hudson-engined Railton used to take off at the top of Brooklands test hill when it was Road Tested.

Psychology

MY interest in the psychological effect of road markings rarely flags, and I am sure that more thought on those lines would lead to greater safety. That thought is being applied is evidenced by the action of authorities in scrapping the three-lane marking and substituting cat's-eyes down the middle of the road.

This very action illustrates what I mean. A driver on a narrow road, trying to overtake but unable to do so, feels convinced that if the road were wider all would be safe and well. Widen that road into a three-lane stretch, marked as such, and he will feel that the three-lane road is a source

of danger. Substitute two-lane marking and he will revel in "plenty of room for anything," thus proving that only the marking makes the difference to the danger sense. His conclusive evidence, if he requires it, comes in the statement of the county surveyor of Buckinghamshire that, so far as he has been able to prove anything by statistics, the wider a road is the safer it is.

Cars and Standards

IT would be invidious to say that owners of certain cars are nearly always bad drivers, but I think most people agree that the sight of one or two makes in the mirror sounds a cautionary note. No such inhibition need bother one about naming cars that are customarily well driven, and I think I would put amongst the top classes the Armstrong Siddeley (no, I do not own one). I have rarely been inconvenienced by the driver of an A.S., or by that of a Daimler. Amongst small cars, the Fiat 500 seems to be very well driven, and I always feel very safe in the proximity of a Citroën. There is a special reason here, I think. The less knowledgeable are intimidated by the thought of front-wheel drive, with the result that the buyers of such cars are usually keen types and good drivers.

In the Rain

THE weatherproofing of sports cars, particularly the older types, leaves something to be desired. When cars are parked *en masse* in the rain attention is drawn to this point by the various methods worked out on the spot to keep out the wet. Draped mackintoshes instead of sidescrims become commonplace, and even rugs do their share—tied at the corners to cover the whole roof and window areas, but leaving the windscreen free.

Thought for Today

TAKEN (without acknowledgement) from my office calendar: "A crank is a little thing that makes revolutions."



From my office calendar.

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With India Tyres on his car, the man who has to cover long distances to a strict time schedule can confidently rely on his tyres holding the road under all conditions and giving the maximum mileage in safety it is possible to obtain.



Jaguar

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STIRLING MOSS

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DAT

PRICE

Total

ENGIN

95.2

press

21.5

WEIGH

per

TYRE

TANK

fuel

TURN

from

MAIN

4ft 2

5ft 0

Overall

gear

ratio

3.78

6.68

11.75

From

30 m.p.

50 m.p.

(by El

Speed

1st

2nd

Top

PET

RESE

LIGH

IGNI

The Autocar ROAD TESTS



DATA FOR THE DRIVER

3.6-LITRE ALLARD

PRICE, with saloon body, £999, plus £278 5s British purchase tax. Total (in Great Britain), £1277 5s.

ENGINE: 30 h.p. (R.A.C. rating), eight cylinders, side valves, 77.8 x 95.2 mm, 3622 c.c. Brake Horse-power: 85 at 3,500 r.p.m. Compression Ratio: 6 to 1. Max Torque: 140 lb ft at 2,000 r.p.m. 27.5 m.p.h. per 1,000 r.p.m. on top gear.

WEIGHT: 28 cwt 2 qr 7 lb (3,199 lb). LB per C.C.: 0.88. B.H.P. per TON: 50.95.

TYRE SIZE: 6.25 x 16in on bolt-on steel disc wheels.

TANK CAPACITY: 17 English gallons (2 in reserve). Approximate fuel consumption range, 15-19 m.p.g. (18.8-14.9 litres per 100 km).

TURNING CIRCLE: 42ft 6in (L and R). Steering wheel movement from lock to lock: 2½ turns. LIGHTING SET: 12-volt.

MAIN DIMENSIONS: Wheelbase, 9ft 4in. Track, 4ft 8in (front): 4ft 10in (rear). Overall length, 15ft 9in; width, 5ft 11in; height, 5ft 6in. Minimum Ground Clearance: 9in.

ACCELERATION

Overall gear ratios	From steady m.p.h. of	10-30	20-40	30-50
3.78 to 1	sec	sec	sec	sec
6.68 to 1	10.0	10.7	12.1	
11.75 to 1	5.8	6.5	8.8	
	4.5	—	—	

From rest through gears to:—

	sec		sec
30 m.p.h. ..	6.0	60 m.p.h.	23.4
50 m.p.h. ..	15.0	70 m.p.h.	35.0

SPEEDS ON GEARS:

(by Electric Speedometer)	M.p.h. (normal and max)	K.p.h. (normal and max)
1st	20-38	32-61
2nd	40-60	64-96
Top	84/85	135/137

Speedometer correction by Electric Speedometer:—

Car Speedometer	Electric Speedometer m.p.h.
10	8.75
20	19.0
30	27.0
40	36.5
50	45.0
60	54.0
70	63.5
80	71.5

WEATHER: Dry, cool; fresh wind.

Acceleration figures are the means of several runs in opposite directions. Described in "The Autocar" of August 26, 1949.

Except that it is "modern" in the approach, the Allard does not conform to any particular style of appearance. In other words, it is an individualist, as in road feel and general behaviour.

No. 1414: 3.6-LITRE ALLARD SALOON

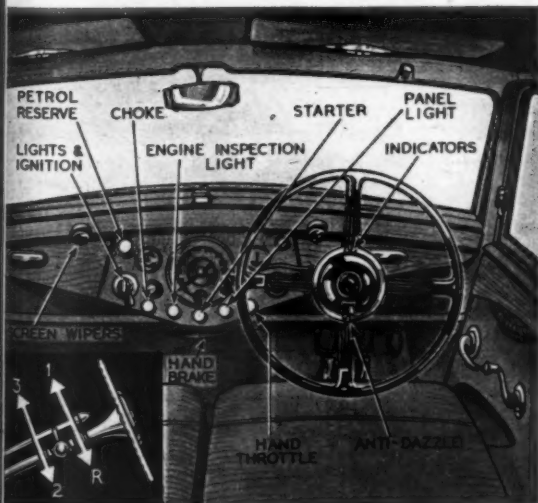
A RECIPE for motoring which is in many ways unusual, even unique for these days, is contained in the Allard. This make in its more potent forms, although produced in relatively small numbers, has put itself very firmly on the map in the post-war years by notable and numerous successes in competitions of the kind in which keen private owners engage. A good deal in consequence of these successes it has attracted much attention abroad as well as at home among members of the motoring community who are appreciative of a highly individual car capable of high performance.

The Allard is chiefly known in its 3.6-litre V8 engined version, although this year the J2 sports model open two-seater has been introduced to be fitted at choice with other engines such as the V eight Cadillac for still greater performance potential. Hitherto the car had been seen, except for some examples of special bodywork, in open two-seater and drop-head coupé form, and only this year was a full-scale saloon put into production. It is an example of this that has now come under review.

It is not necessarily logical or necessary that performance comparisons should be made between the saloon and the open and coupé versions which have hitherto been tested in this way by *The Autocar*, although such a comparison will inevitably be made by some buyers. Unavoidably, the weight goes up appreciably with the saloon body in spite of aluminium panelling, but the closed car still retains a striking acceleration performance as well as the main features of appeal which have caused the Allard to become established as a serious make.

In the first place relative simplicity of design is an attractive feature and one of the unusual points in a car capable of high performance. The Ford V8 side valve engine that is used for the saloon has an excellent reputation for reliability. It gives sufficient power to be useful in a car which is still not of high weight, and as other Ford components are embodied in the chassis the Allard has the great advantage that world-wide servicing facilities as part of the Ford organization apply to it. Throughout, this car is simple and rugged, gaining its results through a useful power-to-weight ratio deriving from moderate total weight rather than from sheer power output. Because of this favourable ratio it can be successful with a three-speed gear box, the high second gear offering up to a genuine 60 m.p.h.

The most material factor governing the road behaviour is the use of high gearing, with the result that the Allard has that exceptional feeling of ease and lack of effort which come only from an engine of ample size operating at extremely moderate r.p.m. A top gear only slightly lower than 3½ to 1 gives a cruising 70 m.p.h. at less than 3,500 r.p.m., an engine speed which can be maintained indefinitely, it would seem, without the car being in the





In the styling of the nosepiece this car is particularly bold. Although the bonnet is of modern one-piece type it has conveniently operated external catches, one of which incorporates a safety catch.



The rear part of the body is particularly clean, and there is no excessive overhang. Wheels are not enclosed.

ROAD TESTS

continued

least stressed. It is very noticeable on journeys of some length that the driver finishes with a particular sense of ease and well-being, for he has not been fussed by a busy engine, he has had a minimum of gear changing to perform, and always there have been the power and the speed to suit the traffic and road conditions.

Sheer maximum speed is, wisely—as a thought of general application—not claimed to be particularly high and the saloon would not readily see more than 85 m.p.h., readings over the genuine 80 mark needing, indeed, some appreciable stretch of clear road. The great point of the car is the ease with which it gets up to its 60-70 m.p.h. cruising rate and holds it. The getaway from rest can be snappy, though the high ratios are felt in conjunction with a centrifugally loaded clutch. In the range from about 30 to 50 m.p.h. there is real punch if second gear is used for maximum effect.

By contrast top gear can be used nearly all the time. As a demonstration the car can even be started on top, in spite of the high ratio, and it will trickle down to 4 or 5 m.p.h. smoothly on top gear and accelerate therefrom. It can weave through town traffic and round right-

angle turns, or regain the cruising rate rapidly after a check, without a change down. Ordinary main-road gradients are swept at speed and a swift climb can be made of the 1 in 6-7 order of hills on second gear. No pinking or running-on tendency was experienced on the comparatively low-octane Pool petrol.

An integral part of the delightfully easy point-to-point performance of the car is its road holding and stability.

A rather striking compromise has been obtained in a suspension which takes up shock well, yet has really no lateral give at all, meaning that the car can be poked round bends and also quite acute turns with maximum enterprise, and yet no heel or sway occurs. It is not the softest kind of suspension for general riding that gives this effect, as might be expected, but there is no real harshness or any pronounced vertical motion on fair to moderately poor surfaces. Whilst the divided front axle design which has been previous practice on the Allard is still used, coil springs are now employed in place of a transverse half-elliptic spring formerly, and these give a softer effect than in earlier models without letting the car become at all spongy. At the rear a transverse half-elliptic spring is still used.

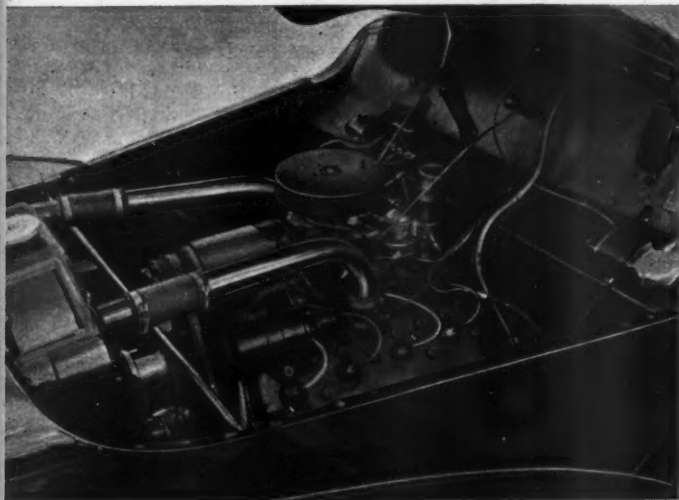
The steering is light for general driving and remains reasonably so down to manoeuvring speeds, is definite in the way that is required for confidence at speeds of 70 m.p.h.-plus, has decisive castor action, and is not subject to reaction from the road wheels to any extent that matters. The Lockheed hydraulically operated brakes are in keeping with the performance. Moderate pedal pressure is sufficient for all ordinary braking and there is an excellent reserve of braking power for emergencies, still without excessive pedal pressure being required.

Positive Gear Change

A steering-column gear lever is used and has one of the best, that is, tautest and most positive, movements experienced with this form of lever, such a result being easier to obtain when only three forward speeds are employed. The lever movements are not particularly light, but the travel is short and there is no uncertainty about obtaining the different gears. The first to reverse movement, and vice versa, is as good as could be wished. A driver has to accustom himself to the fact that the gear positions are different from those normally expected with a three-speed layout; that is, top gear position is away from the driver or upward, instead of towards him. At the furthest away setting of the telescopically adjustable steering wheel there is very little clearance between the gear lever and the wheel rim in reverse position.

The driving position bears evidence of the influence of drivers of sports-type cars. The driver sits low but well

The V8 engine is neat as regards wiring and pipes, and most of the auxiliaries, including the 18 mm. sparking plugs are accessible, there being plenty of room in the engine compartment. A lamp on the bulkhead can be switched on from the fascia to provide useful illumination.



up to a large spring-spoked wheel, which is at just the right angle, and is supported amply in the back and shoulders by the nearly vertical back rest of a separately adjustable seat. Slight curvature of the back rest would be of value to "hold" the driver better when cornering fast. There is plenty of room for the left foot, but the right foot could do with more feeling of support in operating the pendant style of throttle pedal. A pull-and-push type of hand-brake control is within easy reach, whilst leaving the front compartment clear, and is powerful for holding the car stationary. The front seat cushions and back rest practically meet at the centre, and it would be possible to carry a third occupant on occasion. Although the bonnet is long it is not high or obstructive to driving vision, nor are the windscreen pillars noticed as an obstruction; the right-hand wing (in a right-hand drive car) is within view of an average-height driver and the other wing can be seen by leaning over slightly.

Unusual Detail Equipment

Instrument layout again shows the good influence of the sports-car outlook. The gauges have "honest," circular dials, black, with white markings and needles. They include an engine thermometer, reading in degrees F, as well as an ammeter. There is the now exceptional pro-

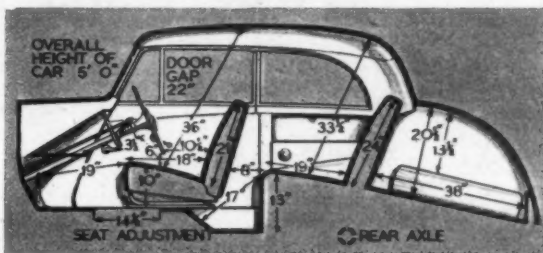
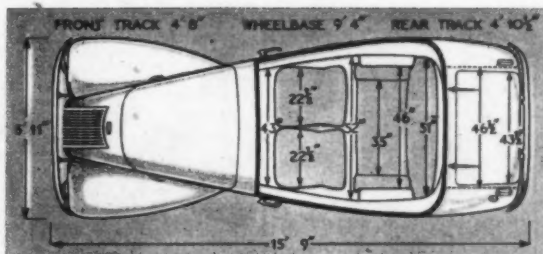


There is useful capacity in the luggage compartment, although the spare wheel is prominent in it, as seen, and the floor of the locker slopes fairly sharply rearward. A useful light in the compartment is illuminated automatically by opening the lid, the struts of which are self-locking.

vision of a control on the fascia for a reserve supply of petrol. A screw-type hand throttle is provided in addition to the choke control, and among the main group of minor controls is a switch for a light in the engine compartment. Other practical detail points have not been overlooked in the saloon body, which gives every impression of being well made, and which has the coachbuilt manner of the doors closing without being slammed. There is a shelf for packages under the fascia, as well as pockets formed low down in the thickness of the doors.

This two-door body is somewhat "occasional," as distinct from "family," as regards the rear seats, although their upholstery in leather over a rubber underlay is comfortable. It would not be easy for the less agile and elderly to get in and out of the rear seats through the space given by tilting the back rests of the front seats, although the leg room is reasonable; there are elbow rests, set at an angle which robs them of maximum value, but a central arm rest is not fitted in the rear seat.

Again unusually nowadays, this unusual car has a windscreen which can be opened by means of a central toggle mechanism, but a sliding roof is not provided. Demisting vents for the windscreen are built in and the car can be



Measurements in these scale body diagrams are taken with the driving seat in the central position of fore and aft adjustment and with the seat cushions uncompressed.

supplied with a heating installation of the type that takes in fresh air from a grille in the nose. This installation is standard for export and an extra for the home market. A degree of control over ventilation which is useful for some weather conditions is given by pivoting panels in the front of the doors, additional to the drop windows.

The excellent detail feature of a rear window blind is included. The view provided by the driving mirror is all but comprehensive. Twin horns give a quite powerful but still pleasing note. The head-lamp beam is satisfactory up to about 70 m.p.h. Always the engine started immediately and required very little use of the mixture control from cold.

Access to the rear seats through the single wide door on each side is by tilting the back rests of the front seats and leg room for back passengers is given by shallow wells. The upholstery is in good leather over foam rubber. The roof light is switched on by opening the doors.





Dundrod Deluge



The mass of the field getting away at the start of the race; Bob Gerard's Frazer-Nash leads A. P. Hitchings' H.R.G., with Mike Oliver's Silverstone Healey, Robin Richards' Healey and Peter Clark's H.R.G. close behind.

Left: Reg Parnell and George Abecassis, with the Aston Martins which finished first and second in the 3-litre class, sending up showers of spray as they pass Tullyrusk cross-roads, the spot known as Deer's Leap.

PICTURES FROM THE
ULSTER T.T. EVENT
(report on pages 990-992)

Stirling Moss, serenely confident, taking Ireland's Corner at high speed despite the rain during his victorious drive; he won the Tourist Trophy, the 3- to 5-litre class, the cup for the greatest distance covered, set up the lap record, and was a member of the winning team.



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First race, first heat, first lap. S. A. Coldham (Cooper) gets away from Paul Emery (Emeryson). In the second row are, left to right, D. F. Truman, E. Winterbottom and Ken Carter, the eventual heat winner.

BRANDS HATCH: ALF BOTTOMS ALL-CONQUERING IN 500 CLUB MEETING

ROLLING MOSS GATHERS NO STERLING

THE 500 Club meeting at Brands Hatch, on Saturday, became something of a Bottoms Benefit once it became clear that all was not well with Stirling Moss' car. The first heat of the first race was the only event in which Alf Bottoms' car was entered in which first place went to someone else—rival Ken Carter.

Moss made his first appearance in the second heat, starting on the last line of the grid because of his absence during practice; a particularly noteworthy absence, for he was, of course, busy winning the T.T. at Dundrod. In four laps, however, he was through the big field to first place, which he held to the end.

In the final Bottoms led from the start, with Moss getting away badly. His engine was now beginning to lose fervour, but his progress through the field was vastly exciting, culminating in passing Ian Burgess on the last corner of the last lap for second place.

In the non-production car race Don Parker, in his J.A.P.-engine special, ran true to form, taking an easy, but well driven, first place to D. F. Truman and J. Wright. In the Brands Hatch Championship, however, principal participants were Ian Burgess, who won the first heat, and Bottoms and Eric Brandon, who were one and two in the second. In the final the by now invincible Bottoms was in front from the start and worked up a useful lead from Burgess, who had passed Brandon on the ninth lap for second place. Ken Carter moved up from sixth at the start to take fourth place.

In the Junior Championship K. A. Gregory and D. N. Brake fought it out throughout the race, battling wheel to wheel on the fifth lap, until, by lap nine, Gregory established himself in front. Alf Bottoms triumphed once more in the meeting championship with a very substantial lead from Carter and Brandon.

PROVISIONAL RESULTS

Lap Distance 1 mile

Open Challenge Race: Heat 1: 1. Cooper-Norton (K. E. Carter) 64.13 m.p.h.; 2. J.B.S.-Norton (A. Bottoms); 3. Cooper-J.A.P. (I. Burgess). Heat 2: 1. Cooper-Norton (S. Moss) 60.10 m.p.h.; 2. Cooper-J.A.P. (A. Brown); 3. Parker Spl-J.A.P. (D. Parker). Final: 1. J.B.S.-Norton (A. Bottoms) 65.4; 2. Cooper-Norton (S. Moss); 3. Cooper-J.A.P. (I. Burgess).

Non-Production Car Race: 1. Parker Spl-J.A.P. (D. Parker) 56.29 m.p.h.; 2. Barton-Turner-J.A.P. (D. F. Truman); 3. Wright-J.A.P. (J. Wright).

Brands Hatch Championship: Heat 1: 1. Cooper-J.A.P. (I. Burgess) 63.35 m.p.h.; 2. Cooper-Norton (A. B. Rogers); 3. Parker Spl-J.A.P. (D. Parker). Heat 2: 1. J.B.S.-Norton (A. Bottoms) 65.15 m.p.h.; 2. Cooper-Norton (E. Brandon); 3. Cooper-J.A.P. (A. E. Brown). Final: 1. J.B.S.-Norton (A. Bottoms) 65.27 m.p.h.; 2. Cooper-J.A.P. (I. Burgess); 3. Cooper-Norton (E. Brandon).

Junior Brands Hatch Championship: Heat 1: 1. Cooper-J.A.P. (D. N. Brake) 60.12 m.p.h.; Cooper-J.A.P. (T. Leigh); 3. Cooper-J.A.P. (A. J. Nurse). Heat 2: 1. Cooper-J.A.P. (R. Montgomerie-Charlton) 61.02 m.p.h.; 2. Cooper-J.A.P. (A. W. Richards); 3. Cooper-J.A.P. (G. H. Symonds). Final: 1. Cooper-J.A.P. (K. A. Gregory) 62.15 m.p.h.; 2. Cooper-J.A.P. (D. N. Brake); 3. Cooper-J.A.P. (R. Montgomerie-Charlton).

Championship of the Meeting: 1. J.B.S.-Norton (A. Bottoms) 64.64 m.p.h.; Cooper-Norton (K. E. Carter); 3. Cooper-Norton (E. Brandon).

PRESCOTT POSTSCRIPT

THE following are the results of the final Prescott hill-climb meeting, held in excellent conditions on September 10.

RESULTS

Sports cars:

Up to 1,500 c.c.: 1. H.R.G. 1,496 (G. A. Ruddock), 53.01s; 2. Cooper Sports 1,450 (E. M. Mackay), 54.07s; 3. Bugatti 1,496 (J. M. James), 55.06s.
Up to 1,500 c.c. 2: 1. M.G. 939 (A. G. Baker), 52.02s.
1,501 to 2,000 c.c.: 1. Frazer-Nash-B.M.W. 1,998 (R. C. Willis), 50.40s; 2. Frazer-Nash-B.M.W. 1,971 (R. F. Peacock), 52.36s; 3. Aston Martin 1,950 (R. E. Darby), 53.16s.
Over 2,000 c.c.: 1. Allard 5,430 (S. H. Allard), 39.38s; 2. H.R.G.-Mercury 3,917 (A. W. Francis), 51.28s; 3. Allard 4,375 (G. M. Mansell), 51.70s.

Racing cars:

Up to 150 c.c.: 1. Iota Tiger Kitten II (H. C. Limes), 48.07s; 2. Cooper (A. B. Rogers), 48.14s; 3. Cooper (P. J. Collins), 49.36s.
151 to 1,199 c.c.: 1. Cooper 996 (K. Wharton), 48.67s; 2. Kieft 1,096 (M. A. H. Christie), 47.20s; 3. Cooper 1,096 (R. E. Bradnock), 49.02s.
1,201 to 1,599 c.c.: 1. Cooper 1,200 (P. J. Collins), 47.25s.
1,601 to 2,999 c.c.: 1. R.R.A. 1,990 (Raymond May), 46.67s; 2. Bugatti 1,690 (P. Mould), 47.64s; 3. Alfa Spl 1,970 (J. B. Norris), 49.02s.
Over 3,000 c.c.: 1. Alfa Romeo 3,600 (R. D. Poore), 43.38s; 2. Allard 5,700 (S. H. Allard), 45.44s; 3. A.J.B. 4,425 (A. J. Butterworth), 47.62s.

Ten Fastest

1. Alfa Romeo 3,600 s. (R. D. Poore)	43.38s
2. Allard 5,700 (S. H. Allard)	45.44s
3. R.R.A. 1,990 s. (R. May)	46.67s
4. Cooper 996 (K. Wharton)	48.67s
5. Kieft 1,096 (M. A. H. Christie)	47.20s
6. Cooper 1,200 (P. J. Collins)	47.25s
7. Bugatti 1,990 s. (P. Mould)	47.64s
8. A.J.B. 4,425 (A. J. Butterworth)	47.62s
9. Bugatti 2,261 s. (P. J. Stubberfield)	47.94s
10. Iota Tiger Kitten II 496 (H. C. Limes) ..	48.07s



By far the fastest-ever Kieft ascent of Prescott was made by Michael Christie in his 1,100 c.c. J.A.P.-engined version.

Below: Denis Poore, complete master of the Alfa Romeo, recording fastest time of the day at Prescott, and (inset) fondling his important right shoe as he chats to John Cobb in the paddock.



NEW CARS DESCRIBED

Sunbeam-Talbot 90

With Bigger Engine

INDEPENDENT FRONT SUSPENSION : HIGHER GEAR RATIOS : DETAIL REFINEMENTS

TO have gained high awards for three years' running in that most gruelling contest, the Alpine Trial, is a feat which focuses attention upon the mechanical design of what is one of the most dainty looking of British sports cars, the Sunbeam-Talbot 90. It is therefore particularly interesting to record that the Sunbeam-Talbot 90 for 1951, which was illustrated in the September 15 issue (pages 976-7), will have certain major developments.

Although the coachwork remains unchanged, the chassis frame has been



Tools are recessed into a curved tray which folds down into the lid of the large luggage locker.

redesigned, independent front suspension has been fitted, and the size of the four-cylinder o.h.v. engine is increased from 75 x 110 mm (1,944 c.c.) to 81 x 110 mm (2,267 c.c.). This is an increase in cylinder capacity of 16½ per cent, and the new engine develops 70 b.h.p. at 4,000 r.p.m., as compared with the 64 b.h.p. at 4,100 r.p.m. previously.

Substantially the main features of the engine, other than the cylinder bore, have not been much changed, and include such points as a three-bearing counterweighted and balanced crankshaft, light-alloy connecting rods, Lo-Ex alloy pistons, chain-driven camshaft with special harmonic cams, push-rod-operated overhead valves in a detachable cylinder head with separated inlet ports, and downdraught carburettor with large air silencer and cleaner fed from a duct leading to the front of the car. One special feature is a new and more efficient water pump, in the upper part of which a block type of thermostat is included, thereby avoiding the employment of exterior water pipes. The front engine flexible mounting has been modified to improve insulation.

In conjunction with the larger capacity engine the final drive has been

changed to a hypoid bevel gear. This gives a direct drive ratio on top gear of 3.9 to 1 instead of the previous 4.3 to 1, and the indirect gears are also slightly higher, as shown in the specification. This four-speed gear box has a baulking ring type of synchromesh on all gears except first, and is controlled by a lightly operating lever at the left side of the steering column, which is housed in a neat fairing, blended into the instrument panel. The change in the engine capacity has added no more than 84 lb to the unladen weight of the car.

Naturally the new type of independent front suspension adopted is of particular interest. It embraces the use of lateral upper and lower wishbones. The lower wishbones are long, and are hinged close to the centre of a large box-section cross-member. They are triangular in plan view, and the hinge bearings are spaced wide apart to provide longitudinal rigidity. They are inclined rearward at the extremities so that the axis of the hinge on each side, if prolonged, would meet in the centre of the car.

The upper wishbones are shorter, and are mounted on hollow brackets above the ends of the cross-member, and the top of the bracket forms an abutment for the large coil suspension spring on

each side. The foot of the spring rests in a pan attached to the lower wishbone. The outer ends of the wishbone are linked together by a yoke which carries the swivel pin for the stub axle of the front wheel. Armstrong double-acting hydraulic spring dampers are employed, and also a torsion bar couples the two sides of the suspension system together and controls roll. Rubber bushes are fitted to prevent the transmission of road noise to the car.

A new high-efficiency variable ratio steering gear has been adopted. The half-elliptic rear springs have also been redesigned, with longer and wider spring blades. Armstrong dampers are fitted at the rear, and a transverse link between rear axle and frame prevents side sway. This suspension as a whole has been designed to give more easy riding over bad surfaces, but also to enhance stability when the car is at speed on good roads.

There is a slight change in the frontal appearance of the car which might pass unnoticed. The front wings have been altered so as to raise the height of the inbuilt head lamps by 3 inches, and the pass lights have been abandoned in favour of small inbuilt side lamps below the head lamps. The head lamps are slightly wider apart than formerly.

SPECIFICATION

Engine.—4 cylinders, 81 x 110 mm, 2,267 c.c. (3.1875 x 4.33 in, 132.2 cubic inches). Overhead valves operated by push rods, counterbalanced crankshaft in three steel-backed bearings, light alloy connecting rods, three-bearing camshaft with harmonic cams, pressure lubrication with detachable by-pass oil filter, downdraught carburettor with automatic choke and air silencer, pump and fan water cooling with selective flow and thermostat control, and flexible engine mounting. 70 b.h.p. at 4,000 r.p.m.

Transmission.—Dry single-plate clutch with chain linkage to pedal. 4-speed synchromesh gear box with steering column control. Ratios, first 13.9, second 9.633, third 5.81 and top 3.9 to 1. Open propeller-shaft with hypoid bevel final drive and semi-floating axle shafts.

Suspension.—Independent front with wishbones, coil springs, and torsion bar. Half-elliptic rear with anti-sway link. Armstrong

double acting hydraulic spring dampers.

Brakes.—Lockheed hydraulic with two leading shoes; mechanically operated hand brake. 10 in diameter drums.

Steering.—Burman high efficiency variable ratio worm and nut with re-circulating balls.

Fuel System.—10-gallon tank. Mechanically operated pump.

Electrical System.—Lucas 12-volt with 35 ampere-hour battery. Twin tail lamps and twin reversing lights. Self-cancelling Trafficators, twin screenwipers, and dual wind-tone horns.

Jacking.—Four-corner mechanical. **Wheels and Tyres.**—Dunlop E.L.P. 5.50 x 16 in on balanced detachable disc wheels.

Main Dimensions.—Wheelbase, 8 ft 1½ in. Track (front) 3 ft 11½ in; (rear) 4 ft 2½ in. Overall length, 13 ft 11½ in; width 5 ft 2½ in; height 5 ft 0½ in. Ground clearance 6½ in. Turning circle 36 ft. Unladen weight with fuel and water 25 cwt 3 qr 21 lb.

The pleated upholstery is shown in this coachbuilders' impression of the rear compartment.



A knurled wheel provides for adjustment of the driving seat, which is of bench type

Improved Humber Hawk

LARGER ENGINE AND I.F.S. — SUSPENSION FOR
BAD SURFACES — THOROUGH VENTILATION

IN order to enhance the performance of the popular and modern family-type of saloon, the Humber Hawk, an engine of larger capacity will now be fitted. The car was illustrated on page 1978 (September 15). Originally the size of the four cylinders was 75x110mm (1,944 c.c.), which produced 56 b.h.p. at 3,800 r.p.m. The new four-cylinder engine follows broadly the same lines, but the size is 81x110mm (2,267 c.c.), which is an increase in cylinder capacity of 16.5 per cent, and it develops 58 b.h.p. at 3,400 r.p.m. The lb per c.c. figure is now 1,259.

This increment should add considerably to the lively nature of the road performance, particularly in acceleration. There are several important changes of detail in the design of the new engine; for instance, light alloy connecting-rods are fitted with the object of reducing reciprocating weight, and bronze bushes are no longer necessary for the bearings of the gudgeon pins. The crankshaft is of course counterweighted, and runs in three large steel-backed bearings. A water pump of increased efficiency is fitted, and the radiator has been made of larger capacity. The thermostat is now housed in a block in the cylinder head jacket and no exterior pipes are necessary for the by-passed water. The oil pump of the lubrication system draws its supply through a floating filter to avoid picking up sludge, and in addition to the pressure feed to the major working parts a special jet plays oil on to the timing chain, which is now fitted with a blade type of automatic tensioner.

Stouter Shaft

To deal with the increase of engine torque a propeller-shaft of larger diameter has been fitted; the gear ratios, however, remain unaltered. The Hawk has the baulking ring type of synchromesh mechanism, which in conjunction with a well-designed steering column control gives a particularly light gear change. Research work has resulted in the introduction of new half-elliptic rear springs and these in conjunction with revised hydraulic damper settings and extra low pressure tyres, size 6.40x15in, have resulted in greatly improved suspension, especially when traversing bad road surfaces. Another improvement relates to steering. The high efficiency variable ratio gear is retained, but the gear ratio has been raised, so that a quicker and more positive response is obtained.

There are several detail improvements to the bodywork. The doors have been given double, instead of single, knuckle hinges to increase rigidity of support and ease of operation. The sun visors have been made wider and improved to provide better protection against glare. To meet the needs of different regulations overseas, the number plate arrangement has been modified with a central

light overhead and provision to accommodate plates of various sizes. The air conditioning and heating equipment, which is supplied as an extra, has been improved in efficiency and in quietness of operation.

The Humber Hawk is a modern design of saloon car, gracefully styled and not flamboyant. It has thoroughly practical equipment. For example, it has a sliding roof, as well as triangular panels in the windows, which can act as extractors or swing wide open to make air scoops—very useful in really hot weather. Grilles are provided on each side of the scuttle, to which conduit pipes from the front of the car can be coupled when an air heating and conditioning system is fitted. Also a ventilator in the top of the scuttle is fitted, so that control of ventilation is particularly comprehensive. The wide windscreen is of curved glass, the roof lining is washable, the running boards are concealed by the bases of the doors, the doors stay open when fully opened, which is a great convenience, the door pulls are also padded armrests, and there is a wide shelf behind the squab of the rear seats. The front seat is of the bench type, and has a flush-fitting central arm rest, and the hand brake control descends from underneath the scuttle and is convenient to reach and to use. The speedometer is in the form of a large half-moon styled into the centre of the instrument panel, and the luggage locker is of exceptional capacity, with the spare wheel located at one side.

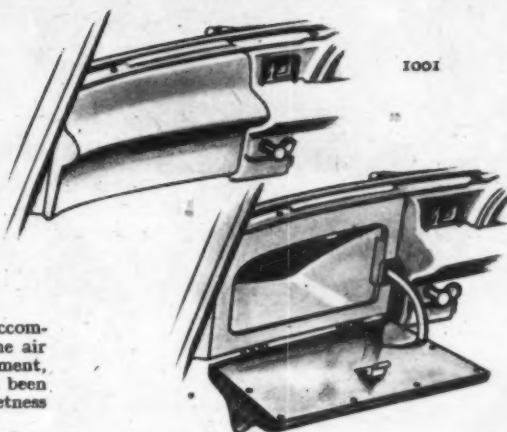
Altogether the Hawk is a car of wide appeal and exceptional merit, and one looks forward to experiencing the difference made to performance by the larger engine. There is choice of six colour schemes.

HUMBER HAWK SPECIFICATION

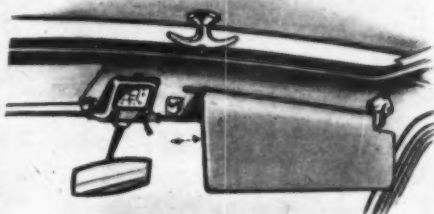
Engine.—4 cylinders, 81x110 mm, 2,267 c.c. (3.1875x4.33in, 132.2 cubic inches). Side valves, aluminium alloy cylinder head, full length water jackets, counterbalanced crankshaft in three steel-backed bearings, light alloy connecting rods. Pressure lubrication with detachable by-pass oil filter and floating intake filter. Downdraught carburettor with automatic choke and oil bath air cleaner. Flexible engine mounting. 58 b.h.p. at 3,400 r.p.m.

Transmission.—Dry single-plate clutch with plate linkage to pedal, 4-speed control ring synchromesh gear box with steering column control. Ratios, first 16.91, second 11.24, third 6.77 and top 4.55 to 1. Open

A large scuttle ventilator adds to the fresh air propensities of the Hawk, which also has a sunshine roof.



Unusual fascia ridge is none the less the lid of a cubby-hole, which folds down to make a handy tray.



Large arc sun visors can be unhitched at the inner ends and swung round to blank side windows. Note the clock above the mirror.

propeller-shaft to hypoid bevel rear axle with semi-floating shafts. Compression ratio 6.32 to 1.

Suspension.—Independent front with wishbones and coil springs. Torsion bar coupling to Armstrong double-acting hydraulic dampers. Half-elliptic rear springs.

Brakes.—Lockheed hydraulic two-leading shoe. Inverted lever operated mechanical hand brake. 9in drums.

Steering.—Burman high-efficiency variable ratio.

Wheels and Tyres.—Dunlop 4.50x15in on detachable disc wheels.

Fuel System.—10-gallon tank. Mechanically operated fuel pump.

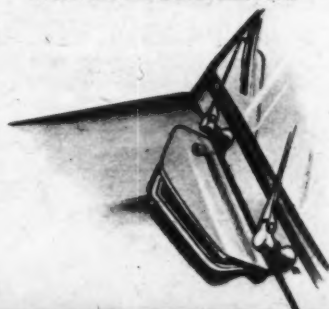
Jacking.—Four corner geared mechanical pillar type, fitting into sockets.

Electrical System.—Lucas 12 volt with 51 ampere-hour battery. Constant voltage control dynamo. Twin tail and twin stop lights. Self-cancelling Trafficators, dual wind tone horns.

Main Dimensions.—Wheelbase, 8ft 9jin. Track (front) 4ft 10in; (rear) 5ft 1in. Overall length, 15ft 7jin; width, 5ft 10in. Height, 5ft 4jin. Ground clearance, 7jin. Turning circle 40ft 6in. Unladen weight with fuel and water 25 cwt 1 qr 18 lb.

Price.—£625 plus British P.T. £174 7s 3d; total £799 7s 3d.

Triangular panels in the front windows can be opened sufficiently to provide scoops for hot weather motoring.



Early morning in Kitzbühel.

TO AUSTRIA AGAIN

POST-WAR TOURING IS QUITE
POSSIBLE IN THE GAY REPUBLIC

by CHARLES G. G. GORDON



IT was early in 1949 that holiday travel in Austria was first allowed again after the war, but few people took their holiday there last summer. There is no doubt, however, that many more will do so in future years, and the experience of our party of four, motoring to Austria last year, may help those who intend to go in future. The Austrian State Tourist Department, from their offices at 22, Princes House, 190, Piccadilly, London, W.1, will provide literature in English, well produced and illustrated,

covering the principal tourist centres and giving lists of hotels.

To go to Austria it is at present necessary to obtain a Military Permit and for this application has to be made to the Military Permit Office, 25, Princes Gardens, London, S.W.7. Just before leaving we obtained from our bank travellers cheques made payable in France, Switzerland and Austria. A permit for Swiss francs has to be obtained from the Swiss National Tourist Office, 458, Strand, W.C.2. This should be applied for some months before the holiday so that one is early in the queue for this precious currency.

It is about 730 miles from Calais to the Austrian frontier, and our route was via Arras, Chateau Thierry, Troye, Langres, Belfort, Basle, Zurich and Sargans, and the roads are good. This route through France avoids much of the





Salzburg, city of Mozart and a world-famous festival of music.

pavé which will be met with on any road keeping farther north. There is no need to book hotels before starting the holiday; we found that if we finished our day's motoring by 6 p.m. we could then, without any difficulty, get accommodation. This applied in France, Switzerland and Austria.

Feldkirch was the first town in Austria of any size, and here we called at the travel agency to do the coupon changing which was at that time necessary. Here and everywhere else we found people most helpful, cheerful and willing to do all they could to make our stay a pleasant one. If your German is not very good (ours was not), it is quite easy to find young people who speak very good English, and who seem to enjoy doing so. Every other man we spoke to seemed to have been a prisoner of war in England and could speak some English.

Over the Watershed

For the next 30 miles after Feldkirch we were slowly climbing to cross the Arlberg Pass, which rises to 5,500 feet. The road was of reasonable gradient but in some places not tarred and therefore bumpy and dusty. There was a good deal of second gear running but we made it without any difficulty. The crest passes through rocky and bare mountains which are able to support, however, some very choice small cows which were keen on our company and as friendly as pet dogs, showing their appreciation by licking our faces. At this point we had crossed the watershed between the Rhine and the Danube. The upper waters of the River Inn rise near here and join the Danube some 250 miles to the north-east at Passau.

Proceeding farther to the east we soon passed through the bare mountains and slowly descended. The narrow valley was closed in by steep slopes on each side covered with woods and grass, so steep as to seem to be unworkable, but every little bit of grass, when cut for hay, is rolled down the hillside and harvested. The road, narrow and winding, follows closely a rushing mountain stream, the bed strewn with rocks, linking a series of villages. The chalet-like houses in the villages are bright with their newly varnished wood and clean white panels, and decorated with geraniums, petunias and other gay flowers in window boxes and outlining balconies. The effect is vividly colourful and most stimulating to the tourist.

We had covered about 100 miles; another day was closing, so we decided to stop at Landeck. The hotel was

an old *Gasthaus* similar to many we had passed. It was a large, solid stone structure with a gabled front and long, straight roof. The entrance steps take one to the centre of the first floor, a large hall with stone floor and rooms leading off all round. Just the place to shake the snow off your clothes and boots after coming in on a cold winter's night. A simple staircase leads to the bedrooms on two floors above. The bedrooms have double glass windows and each is provided with a heating stove consisting of a small fire space and ash pit built into a large structure. This structure, standing on the floor and rising nearly to the ceiling clear of the walls, is either cylindrical or square in shape and covered with tiles. Although it was quite warm in July this was evidence of the conditions to be expected in the winter.

We were awakened in the morning by the sound of many tinkling bells and saw goats and cattle being driven by a small boy through the village to go to the mountain pastures. Each cottage has its goats and the herd grows as it passes along the street; each villager opens his gate and lets his goats out to join the throng. In the same way, when they return at night a few stop at each cottage and are driven into their night quarters.

Industry

The run from here to Innsbruck is first of all through an industrial area of ironworks and factories, and we passed a large barracks and a fine modern hospital. Still keeping close to the River Inn, first above it and then beside it, we soon reached Innsbruck, standing at one of the cross-roads of Central Europe and 850 miles from Calais. It was now Friday—we had left England on Sunday—and, being the week-end, the hotels were rather full, but with the assistance of an Austrian lady who spoke very good English, we were able to get accommodation.

This we found at a *Fremdenszimmer* (a house which lets rooms only). There may or may not be a restaurant attached, but as they are so plentiful it is not important. The rooms in the *Fremdenszimmer* were as we had found all hotel rooms in Austria, scrupulously clean with scrubbed floorboards and loose mats, plenty of water (not always running) and very reasonable, the room costing from 2s 6d to 4s a night. There was no shortage of food even in 1949; meat was plentiful and a lunch or dinner cost about 6s including beer.

The old part of the City of Innsbruck consists of massive



TO AUSTRIA AGAIN . . . continued

In Kitzbühel, the famous mountain resort.

buildings surrounding squares or bordering the streets. The pavement runs underneath the first storey, the shop windows being set back, and opens through arcades on to the street. These walks are hung with attractive wrought iron signs and the shops are gaily lighted and well stocked. The windows above are shuttered and many have window boxes bright with flowers.

The Royal Palace in Innsbruck is well worth a visit—the guide explains everything in English, German and French. It is in much the same condition as when lived in by the Hapsburgs just before the 1914-1918 war. The Hofkirche adjoining was built by Maximilian as his own shrine. Surrounding the central tomb in which his body was to be placed are bronze statues, larger than life, of the celebrated kings who preceded him, placed there to form a permanent bodyguard for his tomb, and many made to hold candles. Among them was King Arthur of England. These statues display, in delicate workmanship, the most carefully wrought features and clothes.

Cenotaph

In the gallery overlooking the tomb are smaller statues, also of exquisite workmanship, representing the saints—Richard I of England included—and also busts of all the most important Roman Emperors. Strangely enough, despite all this preparation, the body of Maximilian still lies at Wiener Neustadt, near Vienna. The Folk Museum, housed in a fine old house with a central courtyard, contains a large collection of old costumes, furniture and so on, which every visitor should see. National costume is still worn by many men and by the women on Sundays.

The cinema we visited had three fully bookable houses each evening and no one entered or left during the show. As we approached our hotel afterwards there was a crowd of people in the vicinity with two policemen in the centre, and it appeared that someone had broken the law in some way. There was a very loud-voiced argument proceeding in quite good temper as the crowd slowly moved along. After about a quarter of an hour, with laughter and appar-

ently much good feeling, the party dispersed. We are quite sure that no Austrian policeman ever arrested anyone without apologies and a strong dislike of his task.

From almost anywhere in Innsbruck you see towering above the roofs the mountains, which rise precipitously from the River Inn, here a mighty rushing torrent. It is possible to travel to the mountain tops by railway. The Hungerburg Railway—two carriages suspended on cables run one up and one down a railway track rising at about 40 deg—takes one to a residential suburb. From here a cable railway, consisting of a cabin holding about 20 people suspended from a continuous overhead cable, takes one to Seigrube at 5,700 feet, and a further overhead cable railway climbs another 1,200 feet to nearly 7,000 feet. The view from here opens up a vista of mountain tops reaching as far as the eye can see.

German Transit

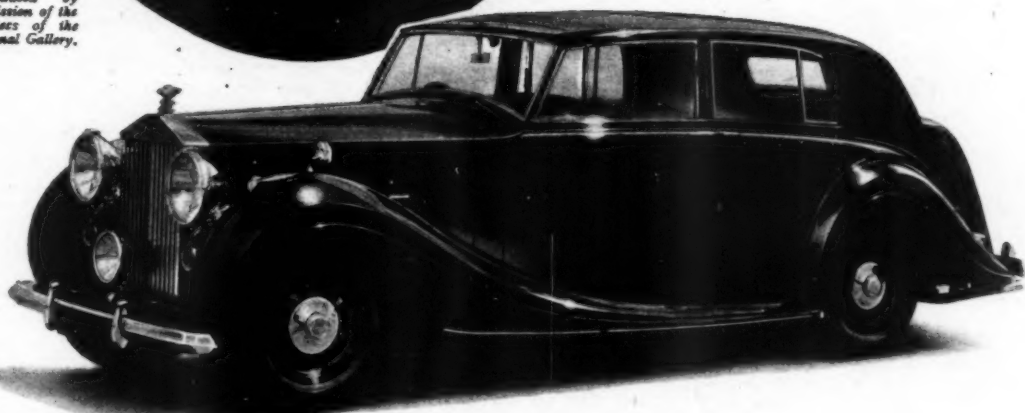
The shortest route to Salzburg is by the Inn Valley to Wörgl through St. Johann and Lofer and across the tongue of Germany which extends south, and we have yet to find a more attractive run than this. A German military transit permit is at present necessary for this journey, which can be obtained in London from the Military Permit Office. A slight deviation took us through Berchtesgaden before reaching Salzburg. The road follows mountain streams through wooded valleys and opens into magnificent parkland set amidst green-sided mountains. The road in Germany is wide and well graded and swings round hillsides to open up most beautiful views. From the town of Berchtesgaden, now an American headquarters, a view can be obtained of the ruins of Hitler's villa and also the mountain top look-out where he consulted his astrologers. A military pass is necessary to visit these two places.

The view of Salzburg, approached from the south, is dominated by the huge castle standing on a hill guarding the plain. The old part of the town below the castle hill and partly surrounded by medieval walls contains many colourful shops offering the best products of the country at



When nature endows an artist with rare genius it also implants an urge of expression, the fruits of which enrich mankind. This jewel of portraiture by Rembrandt, subject Philips Lucasz, is in the National Gallery.

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Motor Show, Earls Court. Oct. 18th-28th

CLEVELAND PETROLS LATER

prices much the same as those in England. Each shop had a decorative wrought iron sign hanging in front, brightly painted and gilded, and bearing a name or representing the business carried on. A close examination revealed intricate patterns representing hours of craftsman's labour.

Lake Resorts

We went on to Wolfgang See and first saw the lake at St. Gilgen—a small, brightly painted village, any corner of which would make a fine stage scene for a musical comedy. We went round the east end of the lake and back along the north side to Strobl. This village, and St. Wolfgang adjoining, are lake-side holiday resorts with plenty of hotels of all sizes, some with gardens on the lake-side provided with boats, bathing huts and diving boards. St. Wolfgang church is gaily coloured inside, has a delicately carved reredos and famous altar paintings. The old pews were inlaid with coloured woods and richly carved, and the tops much burnt by expired candles and bespattered with wax. The water of the lake was clean and warm and a bathe a pleasant prelude to a picnic lunch spread on the green banks. The valleys leading to the lake are dotted with wooden farmhouses and the many paths between them lead to the foot of the thickly wooded hills. In the meadows bordering the streams grow many orchids and also wild cyclamen with the sweet scent of the lily of the valley. Up through the woods rose steep signposted bridle paths leading to farms in the upper valleys.

A further ten miles east is Bad Ischl, a spa with the old Pump Room in the centre of the town providing the usual bad-tasting water to the accompaniment of music. This was as far east as we went—1,000 miles from Calais.

We made the return journey through Salzburg and then south, following the River Salzach through Hallein and Werfen to Zell, another watering place on Zell See. The town is set on a narrow piece of land between the lake and

the mountains, squeezed into short narrow streets running in and out between the old buildings. On the other side of the lake rise, straight out of the water to 6,000 feet, steep green and brown mountains which appear to offer foothold only to sheep and goats. From Zell, running south, starts the famous Grossglockner Pass (8,480 feet).

The road from Zell to Kitzbühel follows the River Salzach at first through a wide valley bounded on each side by an unbroken range of steep mountains which offer no way through. Later, the road climbs the north side of the valley and at 3,800 feet crosses the crest. From here, to Kitzbühel was a slow descent of wild green valleys sparsely covered with gaunt oaks and huge boulders which had stopped precariously on some level spot in their roll down the mountain sides. Kitzbühel, a quiet town in summer, which has its high life in the winter sports season, provided us with the daintiest of cakes and rich ice cream.

City Gates

Rattenberg, a medieval town entered through gates in the old walls, can have altered little for centuries. The tall houses have thick walls covered with faded coloured stucco pierced with small shuttered windows, and shops below open on to the pavement; they are low pitched, and with beamed ceilings. These shops provide the everyday requirements of the country community and are in some cases the workshops of the village craftsman. In one a blacksmith was shaping a leaf for the ironwork for which Austria is so famous.

We had now rejoined the road we used on the outward journey, and without incident or difficulty retraced our route through Switzerland and France to Calais. So ended a long-to-be-remembered three weeks' holiday in Austria. The people are traditionally good hosts, their country has always been the playground of the English, and they are anxious to welcome us again as before the war. We shall certainly be with them again in future years.



At Filsbach, between Zurich and Sargans, the pastures rise up between the woods of a narrow valley.



In the old part of Innsbruck, with its massive buildings, elaborately carved, and the famous golden roof on the 15th-century house on the right.



THE AUTOCAR.

A 12,000ft mountain pass in Colorado (see letter 62892)



CORRESPONDENCE

OPINIONS EXPRESSED ON THESE PAGES ARE THOSE OF OUR CORRESPONDENTS, WITH WHICH "THE AUTOCAR" DOES NOT NECESSARILY AGREE. LETTERS INTENDED FOR PUBLICATION SHOULD BE ADDRESSED TO THE EDITOR, "THE AUTOCAR," DORSET HOUSE, STAMFORD STREET, LONDON, S.E.1.

AMERICAN PASSES

Over 12,000ft in Colorado

[62892].—I have noted that your articles on motoring in America fail to mention any of our high mountain country, as in California or Colorado, with mountain passes of over 10,000ft. I recently made an 8,000-mile tour in four weeks from California to our New England states and traversed two passes in Colorado in excess of 12,000ft. I would like to know if in Europe there are any higher passes in regular use.

My present car, which was used on the above trip, is a 1938 La Salle 125 h.p. Club Coupé; it has steering column gear shift, i.f.s., clean lines, 7.00x16.00in tyres, and can be driven 500-600 miles a day on a trip without fatigue to car or driver. It still has its original paint and has not had a major overhaul, though it has been driven in excess of 70,000 miles and still gives 12-15 miles per gallon. It uses only two quarts of oil in a thousand miles.

Enclosed are photographs of one of the higher passes I mentioned above, and as I have noted you have never published in your Correspondence columns a picture of an "American Alpine Pass," I would be honoured for you to use mine.

San Francisco 23, California.

J. HARVEY NEWBURY.

THE B.R.M.

Reasoned View of the Failure

[62893].—It is most unfortunate and no doubt very disatisfying to many that the B.R.M. failed to get away from the line at Silverstone. It was also disquieting to hear that some fundamental trouble had been found in the engine at this late

stage. I would appeal, however, to some of the younger enthusiasts, not to start running this great project down.

I heard some very harsh and also very stupid things said after Silverstone, sometimes by people who should have known better. A "crisis," even of this magnitude, can happen in the best organized of stables, and, indeed, it is not the first time that such a thing has occurred to a team in the annals of motor racing. I would appeal to these people, particularly when in the hearing of the general public, who may not be so well informed on the vagaries of the high performance racing car and the burning of midnight oil, rather to try to explain the snags and speak hopefully for its future when these faults are overcome.

It is unfortunate that the publicity in regard to the car itself was so badly directed. To call it the best car in the world, before it had been proved that it was, could surely only be tempting Providence.

Let us hope that the B.R.M. will very soon prove itself in an actual race; then Mr. John Bolster will be able to alter some of the words of his ditty and we can all throw our hats in the air.

JOHN JERVIS VIRR.

Dudley, Worcestershire.

Black Side of the Picture

[62894].—I was one of the thousands who went to Silverstone on August 26 for one reason only—to see the B.R.M. race. As a result of the farcical début of this new and much-publicized car I am still suffering from an attack of acute frustration.

When it is considered that: (1) This car has been under way for so long; (2) The public's appetite was whetted in May, after seeing the B.R.M., by being promised that two cars would definitely race in August; (3) Neither of these cars were ready for practice and special dispensation had to be given for one only to practise on the day of the race, its best lap not being as fast as a number of participants in the production car race; and (4) finally, the car failed to move from the starting grid in heat two, and Mr. Raymond Mays is reported to have said that the B.R.M. will not be hurried again—I for one am despondent and resentful at the loss of face we have suffered, and it must be admitted that those responsible for this sad state of affairs have utterly failed and, before the public will continue to subscribe to the B.R.M. funds, some heads must fall.

M. S.

Bawtry, Yorkshire.

National or Not?

[62895].—If the B.R.M. is anything to do with national effort and prestige, then it is a sorry demonstration that we cannot undertake successful car research; but if it is a piece of private fun, then bunging it in like that to take a sporting chance was a jolly good effort!

G. CORDEROY.

London, S.W.1.

Restatement of Faith

[62896].—Through your columns I should like to say how very deeply I appreciate the numerous letters I have received from well-wishers of the B.R.M. after the tragic Silverstone failure. It was a bitter blow to my colleagues and me, and particularly so after the superhuman efforts of Peter Berthon, S. Tresillian and the grand lot of mechanics, who put in an unbelievable number of continuous hours' work in an endeavour to be at Silverstone.

None of us at the B.R.M. works has lost faith in the car in any way, and the many letters I have received, cheering us on, are a fine stimulant to all of us to go ahead to make the B.R.M. 100 per cent successful.

RAYMOND MAYS.

Bourne, Lincolnshire.

CORRESPONDENCE

continued

STARTING FROM COLD

That Thrust-ring Drag

[62897.]—I read with interest the letter [62830] from Mr. Robert L. Preece. I also find that my 1938 Morris Eight will not start by battery if the clutch pedal is down. I wonder if this is peculiar to the Eights, or do other cars also react in like manner?
E. DUKE (Miss).

Clifden, Co. Galway, Eire.

PARVISE

Expert Opinion on the Feature

[62898.]—The word "Parvise" ["Disconnected Jottings," September 1] is derived from the Latin word *paradisus* (late Latin *parvisius* or *paravisus*) and the French *parvis*. Generally it is now used to describe the open space round a church, although it can be used specifically to describe a porch or the room above it. Additionally it may be used with reference to the cloisters or burial place of a monastery. Thus Mr. L. A. Postle is correct, although the meaning of the word is wider than his limited definition.

I hope that this will be of interest.

RICHARD DE Y. BATESON,
M.A., A.R.I.B.A., A.A. Dipl.

Buntingford, Hertfordshire.

READING THE MAP

Welsh Rally an Original Event

[62899.]—I was interested to see letter [62856] from my friend Dr. G. P. Foster in which he eulogizes the various map-reading competitions that have been organized since the war. He ought to know, having been a heavily disguised point marshal in several night trials.

But in his letter he confuses the origin of the forthcoming Welsh Rally in December, which is entirely a Bugatti Owners' Club affair. It will be a copy neither of the pre-war B.O.C. Welsh Trials, nor of the widely imitated H. and B.M.C. Night Trials, but will take a completely original form. Indeed it could hardly fail in this since Dr. Foster himself is taking an active part in the organization.

HOLLAND BIRKETT,

Public Relations Officer, Bugatti Owners' Club.
Prescott, Gloucestershire.

"RUBBER ROADS"

Views of a Pioneer of the Principle

[62900.]—You have published two very interesting articles on Rubber Roads in *The Autocar* of October 21, 1949, and July 28, 1950, and it is gratifying to find so much interest is taken by you in this subject on behalf of the motoring public.

In your article of October 21 you very rightly point out that the term "rubber roads" needs qualifying and with this I fully agree. The term is being very loosely used; you refer to "rubberized asphalt" and "rubber asphalt" as one and the same thing whereas, as the words imply, they are of course quite different, rubberized asphalt being used as a light carpeting for maintenance purposes on worn wood block, concrete or other surfaces and rubber-asphalt used as a permanent road surface which, if properly constructed with a much higher percentage of rubber, requires no carpeting, dressing or other form of maintenance.

Rubber-asphalt surfacing has stood the test of time and severe traffic conditions and you refer to a section which I laid in 1937 which, you say, "is still giving satisfactory service," but you add "Clifton Rise (or Clifton Hill as it was then called) is not a main road and no control strip of ordinary material was laid at the same time—with the result that no standard of accurate comparison exists by which to measure results."

This clearly deserves and calls for an answer, not, if I may say so, on its merits but only in reply to certain propaganda which I find is being spread to decry the use of scrap tyre rubber. I use this rubber solely because I have found by practical experience that it gives better results than anything else; I have no interest in rubber and I write only as a road engineer. I am fully aware from over fifty years' experience in the manufacture and handling of asphalt and asphalt products what happens to ordinary asphalt on a hill or incline.

The type of rubber I use retains its identity throughout the entire thickness of the asphalt, rendering the asphalt non-skid

during its entire life, resists heat and cold, prevents deformation and provides resiliency and all other desired properties. I have not found any other kind of rubber to do the same thing; incidentally, I should be willing to make my experience available to anyone requiring information.

With regard to there having been no control laid by which "to measure results and form an accurate comparison" I would point out that a length of ordinary asphalt was laid adjoining my section of rubber-asphalt; this was taken up and has been relaid, and the surfacing in question today receives attention from time to time—i.e., maintenance. This ordinary asphalt was laid in a much more favourable position than the length of rubber-asphalt, the former being on the level with ordinary moving traffic, whereas the rubber-asphalt is not only laid on an incline but is also subject to turning bus traffic from an adjacent road and horse-drawn coal carts, using skid-pans, from a rail-lepot.

This traffic has continued without interruption for over twelve years; neither the turning buses nor the skid-pans on the carts nor heat and cold have made any impression whatever on the surface of the rubber-asphalt, and nothing has been spent on the work in the way of maintenance since the road was laid.

In addition to what you have so admirably written on rubber roads you suggest that "what is needed now are reports on wear and effect on vehicles, particularly after contamination of the surface, and these will no doubt be eventually forthcoming." The enclosed copies of letters received from three Borough Engineers who have been in office at Deptford since the rubber asphalt was laid will, I think, furnish this precise information.

A. E. H. DUSSEK.

Bromley, Kent.

[The letters mentioned confirm our correspondent's claims for Clifton Rise.—Ed.]

RAIN SPOTTING

Is Maroon a Liable Colour?

[62901.]—I heartily agree with Dr. A. W. O. Taylor [62854] that we ought to get to the bottom of this rain spotting on maroon finishes. I have exactly the same trouble on a Triumph Roadster which has been re-cellulosed. I do not think it is anything to do with the polish that is used. In my case a prodigious amount of sweat and a well-known make of cleaner and polish will (almost) remove the spots, but they come back after the next rainstorm, looking worse than ever. Several people I have talked to about it have all said that maroon is a very bad colour, prone to do this sort of thing. What do the paint manufacturers say? It would be very interesting to know if American finishes of this colour suffer from the same defect.

PATRICK O. R. SCHREIBER,
M.I.Mech.E.

Cobham, Surrey.

T.V. SUPPRESSORS

Not Only Pleasure is Spoiled

[62902.]—Some time ago the Chief Signal Officer at London Airport stated that a single unsuppressed car could play havoc with the bad-weather landing aids.

Car ignition can also interfere with the growing V.H.F. radio communications used by the police, fire and ambulance services and by individual doctors.

ANDREW REID, Radio Industry Council.

London, W.C.2.

PERSONALITY

In the Mind of the Beholder?

[62903.]—Your correspondent Mr. L. W. Iddins [62869] reminds me of my grandfather's axe—it is better than any of these new-fangled things, and is as good as when the old man bought it—only had five new handles and three new heads, too.

Incidentally, some lumberjacks think a favourite axe has personality. They, like Mr. Iddins, are mistaken—it is all in the mind of the beholder. Mr. Iddins' ancestors thought the voluminous nudes of 200 years ago had personality-plus. Mr. Iddins would probably merely think them fat.

Have lathes, mincing machines or mowing machines got personality? Or are they just man-made objects the same as the Austin, which may need only its exhaust welding to change it from being fierce to tame?

G. R. PINNINGTON.

Eccles, Manchester.

CORRESPONDENCE

continued

PERSONALITY

Magic Effect of a Wash and Polish

[62904].—I should like to confirm one point made by Mr. L. W. Iddins [62869]. I have not yet been fortunate enough to own a new car, but it has long been my experience that an old car runs twice as well after a wash and polish, without even opening the bonnet. What's more, if one has decided to part with an old friend in favour of a younger, it will make a special effort to show that there's life in the old dog yet. I have recently made a change, and although the transaction was already settled, I ran the old bus for a week before finally parting with it, and all the rattles disappeared like magic, whilst hills were climbed with almost forgotten spriteliness.

Glasgow.

IAIN P. ORR.

and rough corner to the left just before gaining the summit. I walked up the pass last Saturday to have another look at it. The valley is worth visiting for its wild and cultivated beauty and is best approached from Glangrywny, about two miles short of Crickhowell after leaving Abergavenny.

Hereford.

H. BUTCHER.

CORRESPONDENTS WANTED

Calling Delage Owners

[62908].—I wonder if you could possibly spare space in your columns to say that I would be very glad to hear from any other Delage enthusiasts with a view to swapping "gen," and the possibility of a "get-together" in the future?

Walton-on-Thames, Surrey.

DONALD G. BISSETT.

SPECIAL BUILDING

Request for Advice

[62905].—I have owned many standard production sports cars, and am now considering building a special, possibly powered with a 30 h.p. V8 unit. I had in mind a short, light chassis, sensibly sprung, high-g geared for speed and economy, and with a lightweight, comfortable, two-seater body. The use of standard components would make for ease of maintenance.

As I have had no experience whatsoever of this type of job, I would very much appreciate a few words of advice from any of your readers who have had experience.

Best wishes to your most excellent journal.

277, Barton Street, Gloucester.

E. T. LUNDEGAARD.

VAUXHALL HERALDRY

Separating the Fabulous Creatures

[62906].—May I reply to letter [62866] wherein Mr. R. L. Lisney wonders if there is a possibility of the griffon and the wyvern perhaps being one and the same?

Quoting briefly from the *Encyclopaedia Britannica* on this absorbing heraldic subject, I will endeavour to show that the wyvern is unconnected with the griffon.

Griffon. Of the outlandish monsters the griffon is the oldest and chief. It is probably correct to say that interested readers and correspondents are agreed that this fabulous creature has the head and wings of an eagle, the rear being that of a lion.

Wyvern. Early spelling wiver; M.E. wivere, (viper); O.E. wivre; O.Fr. wivre, mod. vivre. It is a doublet of viper, with an excrement n., as in "bittern." M.E. bitore.

This heraldic monster has the forepart of a winged dragon and the hindpart of a serpent or lizard.

The wyvern, as it was known from the 16th century, takes a new form under the care of the inventive heralds from that period. In the middle ages it was a lizard-like dragon generally with small wings. During the middle ages there seems small distinction between the wyvern and the still rarer dragon, which, with the coming of the Tudors, who bore it in their badge, is seen as a four-legged monster, with wings and tail that ends like a broad arrow. The monster in the arms of Drake, blazoned by Tudor heralds as a wyvern, is clearly a fire-drake or dragon in its origin.

It would seem from the above that the wyvern is a variation of the dragon and although heraldically younger than the griffon, would be as old in folklore, if not older.

It is interesting to note that not until the 20th century was the dragon officially restored as proper only to the British race of Ulster Pedragon, by its incorporation in the armorial bearings of the Prince of Wales. The belief of dragons seems to have arisen without the slightest knowledge on the part of the ancients of the gigantic and astonishingly dragonlike extinct reptiles of past ages.

A. R. KANIA.

Stockton Heath, Cheshire.

MOUNTAIN ROUTES

More Information of the Grwyne Fechan Valley

[62907].—I was interested in Mr. J. J. Woodcock's letter [62863] on the subject of Welsh hill-climbing, being familiar with the districts mentioned. I will concern myself only with the route up the Grwyne-Fechan Valley. In 1933 I encountered the same wall, less than a mile up the valley from the last crossing of the stream, as did Mr. Woodcock, but on that occasion I was motor cycling, and with the help of a pillion passenger to push and to walk when required, I managed to gain the pass summit and to descend via Rhiw Trumau. This, as far as I remember, was rough, steep and narrow in places, and I think a motorist may have some difficulty in getting down. Another descent marked on a one-inch map is to the left along the hillside and down past a farm called Blaenaufry, but I cannot vouch for its quality.

To return to the climb up the pass, most of it consists of a lovely grass road, very firm, and, I believe, constructed by a former Lord Glanusk for driving a carriage and pair. Whoever tries the road with a car, however, must take along some planks, or else motoring is impossible. There is a very steep

BODY INCHES

False Aims in Modern British Design?

[62909].—Letter [62864] refers to British manufacturers squeezing passenger accommodation to obtain the greatest advantage of the short wheelbase on which they are building their popular cars. As a reader of your journal for the past 46 years, and as a foreign owner-driver of British, American and Continental cars since 1911, I should like to say that I am in agreement.

The driver is seated well under the front cowl, thus allowing the rear seat to be moved forward of the rear axle to provide seating accommodation within the wheelbase. This does not compensate for the longer wheelbase, and hence the more comfortable ride obtained with the non-sterling area popular cars.

The question of subordinating comfort, accessibility, and so on to manufacturing methods, and the use of lighter gauge steel, apparently does not enter into the picture. Many of the modern and popular American cars of 29 h.p. compare very favourably in weight with British cars of 18 h.p., despite the longer wheelbase, extra luggage capacity and other essentials to the comfort of the motorist and passengers.

Sutton Coldfield, Birmingham.

A. E. THORNTON.

BY THE BOB

Computing Pumps Do All the Work

[62910].—In the August 18 issue The Scribe raises the question of buying petrol "by the bob," a practice which the motoring public in general seems unwilling to cultivate. Purchasing petrol by quantity is another tradition which the average Englishman seems reluctant to part with, even though the modern computing type of petrol pump is used exclusively in America and is becoming the standard type for dispensing motor fuel in most other countries abroad.

The petrol computing pump indicates not only the gallons but also the total value of the sale down to individual farthings and the price posting can be varied to accommodate changes in the current price per gallon. How much easier to order an even shillingworth than to juggle about with 3s 1½d or some such nonsensical amount.

From the garage proprietors' point of view cars can be served and despatched much quicker and the fractions of gallons occasioned by even money sales do not affect his book-keeping as the computer incorporates totalisers for both money and quantity which keep the daily records for him.

Computing pumps were designed for the convenience of the motoring public and it is largely owing to them that they are not more widely installed. So why not make use of these ingenious devices and request in future "ten bobsworth" instead of "three gallons" or, if you are feeling particularly munificent, "fill her up", and have the pleasure of guessing how much will appear in the windows lettered "This Sale."

Apron slot pumps, these have, of course, appeared in the past but never with any great degree of success. They are open to abuse, and would be marked down for special attempts at "fiddling" in these days, with petrol so valuable a commodity. I think, however, the chief objections would come from the local authorities who are responsible for granting petroleum storage licences. Quite recently the regulations governing the granting of these licences have been considerably tightened up and I do not think that the local authorities would view kindly the possibility of the public having ready access to a potential fire hazard as dangerous as petroleum fuel mixtures.

R. G. SPALDING.

Eastbourne, Sussex.

The Autocar READERS' SERVICE

Locating Body Noises

My 1939 Sunbeam-Talbot saloon has developed trouble with the propeller-shaft at the gear box end, fouling the tunnel when the car is fully laden. This causes a most unpleasant banging over bad roads. The car's suspension, engine mountings, and universal coupling flanges have been checked and are all in order. I should imagine that the sinking of floor boards and tunnel are the cause of the trouble. The transmission tunnel is part of the chassis, which makes it the more difficult to deal with.

Brackley, Northamptonshire.

R. R. V.

THE fault from which your car suffers is certainly curious, and no obvious reason for it presents itself, unless the chassis frame is giving way. The best plan in cases of this sort is to get two or three people in the car, or at least a friend and some ballast, and then try to trace the cause of the sagging while the car is stationary.

Changing Engine Type

After a long search, I have recently acquired a complete 1932 Riley chassis and engine. The chassis is perfect, but I now find that, rather than recondition the engine, it would be cheaper in the long run to fit a Ford engine and gear box.

As I am not a mechanic I don't know if the differential would take it, as it has been running with a 9 h.p. engine, whereas it may not last long with a 10 h.p. Ford engine.

G. H. H.

Cambridge.

THE Ford engine should be quite satisfactory in the Riley chassis, and quite within the stress limits. The only difficulties are likely to be encountered at the universal joint and torque tube mounting, and the front engine mounting.

Anti-freeze Strength

When I put in the appropriate quantity of anti-freeze this winter, a certain amount will inevitably be lost in time, either by spilling through the overflow pipe or by evaporation, and the solution will become weaker when topped up with water. I should like to know of a meter for checking the strength of the anti-freeze solution from time to time, in order to ensure that protection against frost is, in fact, still there.

J. W.

Nottingham.

THE only strength meter as yet submitted to *The Autocar* was one intended for the trade. However, it

Week by week *The Autocar* answers by post many questions from readers on a wide variety of motoring subjects—technical, legal, touring, maintenance, sporting and general. A selection from these of wide interest is published as a regular feature. Readers wishing to use this service should address their queries to The Editor, *The Autocar*, Dorset House, Stamford Street, London, S.E.1. Only a selection of queries and answers can be published, in view of which fact a stamped, self-addressed envelope should be enclosed for a direct reply. It is requested that queries for this section be kept separate from other communications, and that questions which normally are addressed to manufacturers' service departments should not be regarded as eligible.

should be understood that anti-freeze does not evaporate and provided the system is not leaking the level in the radiator should sink to a certain amount owing to spilling through the overflow pipe, but no farther. Should it subsequently leak, then it would be necessary to top it up with correct water-anti-freeze mixture, but if any further losses are simply the effects of water evaporation, topping up with water is quite satisfactory for an indefinite period.

Flag Signals

I would be most grateful if you could let me have a list of the current flag signals used at car race meetings.

Edinburgh.

T. H. L.

THE flag signals used at car race meetings are as follows:—

Black and white check—end of race; black flag (shown with white number)—signal for car bearing that number to stop at the pits on the next lap; white flag—an ambulance or service car is on the circuit; yellow with vertical red stripes—take care, oil has been spilt all over the road; yellow flag (motionless)—take care, danger; yellow flag (waved)—great danger, be prepared to stop; red flag—signal for complete and immediate stop; blue flag (motionless)—other competitor is following you very closely; blue flag (waved)—other competitor trying to overtake you.

SAE Number

I have noticed that most service stations overseas call lubricating oil by its SAE number rather than by its maker's denomination. What is this

SAE number and how is it obtained?

Most English car instruction books quote the recommended oil by its maker's name and type, which means little or nothing in many places abroad.

G. N. B.

West Indies.

THE SAE number is a rating calculated by the Society of Automotive Engineers of America (hence SAE). We suggest, therefore, that you find out from the manufacturers the SAE number of the oil recommended for your car and bear this in mind when travelling in different countries where your brand may not be available.

Overheating

I have just purchased a 1932 Alvis Speed 20 and had this completely stripped and rebuilt. Unfortunately I am troubled with persistent overheating. Radiator and pump seemed in perfect order, but when, as a last resort, I cleaned the radiator with caustic soda there was an immediate improvement. Now it takes about 40 miles to reach boiling point instead of 10 miles as formerly.

Bristol, 7.

W. V. C.

THIS model did run hot but not 40 miles to the boil! You have had the car rebuilt and are, therefore, possibly still running it in, which might account for a good deal of overheating. Make sure, first of all, that the ignition is not retarded or the mixture too weak, both of which can cause overheating. If this fails to do any good, the radiator would certainly appear to be at fault, but bearing in mind your treatment of it it will probably need specialized service or replacement. Make sure that the interiors of the hose connections are sound.

INFORMATION SOUGHT

Correspondence, addressed c/o *The Autocar*, can be forwarded on behalf of readers seeking the following information and handbooks:—

No. 15546.—Wiring Diagram

"J. S. H."—1936 10 h.p. Talbot.

No. 15561.—G & Amilcar

"E. P."—All possible information

No. 15562.—1925-6 10.8 h.p. Rhode

"J. M."—Maintenance hints and a handbook.

No. 15563.—1937 6 cyl. 14 h.p. Lancaster

"F. S."—Maintenance information and a handbook.

No. 15564.—1932 12-50 Alvis

"J. T."—General information and a handbook.

No. 15565.—1930 16 h.p. Light Six Singer

"D. A. B."—General information and a handbook.

No. 15566.—1932 10.2 h.p. Sunbeam Sportsman Coupé

"R. F."—All possible information and a handbook.

No. 15567.—1939 TB M.G.

"J. N."—All available information and a handbook.

No. 15568.—1934-35 12.8 h.p. Sunbeam

"J. M. D."—All possible information; handbook also required.

No. 15569.—1939 10 h.p. M.G. Drophead Coupé

"E. D."—Maintenance hints, general information and a handbook.

No. 15570.—1934-35 12.8 h.p. Sunbeam Dawn

"M. W. P."—Maintenance hints, lubrication chart and a handbook.

No. 15571.—1930 Morris Ten-Four

"J. S. F."—All possible information on maintenance and a handbook.

No. 15572.—1932 International Aston Martin

"G. A. W."—Hints on maintenance running and tuning; handbook also wanted.

No. 15573.—M.G. NA Magnette Tourer

"A. R. W."—Hints and tips on maintenance, possible troubles and a handbook.

No. 15574.—Handbooks Required

"W. G."—1934 10 h.p. B.S.A.

"R. R. V."—1939 10 h.p. Sunbeam-Talbot.

"D. W. K."—1930 Austin Seven.

"D. H. J."—1932 12 h.p. Triumph Scorpion.

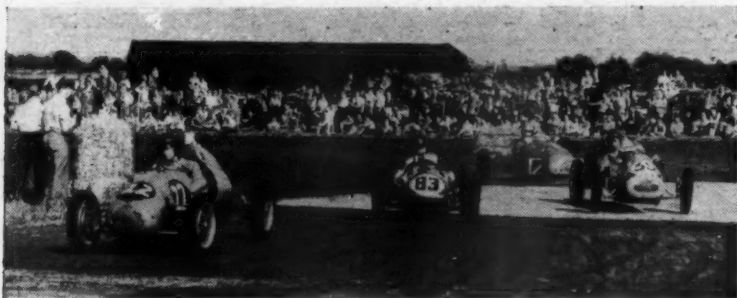
"C. C. F."—1936 12 h.p. Humber.

"H. P."—1934 Standard Ten.

"V. S."—1935 10.8 h.p. Triumph Gloria.

"K. E. C."—1935-36 12.1 h.p. Renault

THE SPORT



Alan Rogers (Cooper) leading the 500s round Cowton Corner at the Yorkshire S.C.C. meeting, at Croft, on September 9.

A GOOD deal seems to have happened since this column last appeared, and most of it will have to be dismissed rather briefly because of that old paradox known as pressure on space. However, things appear to be gradually returning to normal, and—barring wars, lockouts, strikes and divine intervention—matters should now proceed more or less according to plan.

THE weather during the T.T. was enough to damp the ardour of the most fervid enthusiast; the more pity as the race turned out more thrilling than most of us had expected. Moss' handling of the big Jaguar in the wet set the seal on his recent performances with smaller cars; he is unquestionably the most instinctively perfect driver that this country has produced since the days of Dick Seaman, and in another year or two should be a Grand Prix driver of the very highest class—and I mean the highest, bar none. But I do not agree with those who say that he should drive the B.R.M., or something equally fast, yet awhile; he could do it, and beat most people, without any doubt, but years of experience count, and many promising drivers in the past have lost their lives through trying to do too much too quickly.

With regard to the future of the race as a whole, it is absolutely necessary

that more effort is made next year to attract Continental opposition, as a British victory in the present circumstances means nothing whatever, especially abroad. The R.A.C. should realize this and be prepared to abandon their present attitude of apparent unconcern; even to the extent, if necessary, of obtaining sponsorship for the race to provide the starting money without which no one, whether British or foreign, should be expected to race in these penurious days. And they could also, with advantage, be prepared upon occasion to emulate Nelson and moderate their present attitude of rigid adherence to technicalities—after all, it is their plain duty to do all in their power to help entrants (however undeserving they may sometimes seem) and render their paths as smooth as possible.

THE August 26 Silverstone meeting was full of surprises, alarms, excursions, and what have you. Not that the B.R.M. business came as a surprise to some of us, though nobody could have expected quite such an enormous brick to be dropped; unfortunately the inordinate delay, coupled with a wealth of rumour and counter-rumour, had long given indication that all was not well in that camp. I personally have considerable faith in the basic design of the B.R.M., and sincerely hope that next season it will fully justify itself in the eyes of the world; but this will never happen unless some drastic reorganization takes place, whether it wins at Goodwood or not.

The new booklet, "B.R.M.—Ambassador for Britain," which has been published by the *Daily Express* in aid of the B.R.M. Fund, contains a number of excellent photographs and several interesting articles which are worth reading. But the history of the project is rather sketchy; a guarded account of some of the troubles which were met and overcome would have been of great interest to the enthusiast.

THE biggest surprise of the meeting was the terrific practice lap put in by Johnnie Claes at 92.85 m.p.h., equal to the best Alfa lap either in practice or in the race itself. Moreover, he did it several times to show that there was no fluke about it; with the big Talbot this was really good, and it is a great pity

Conducted by S. C. H. DAVIS and J. A. COOPER, A.M.I. Mech.E., M.S.A.E.

that he crashed on the first lap of his heat, before he had a chance to demonstrate his ability. Johnnie is, of course, Belgian, but has spent a good many years in this country (remember his dance band, the *Clap pigeons*?); he is not only a very pleasant, unassuming person, but also has served his novitiate in racing in a sensible manner and is rapidly becoming a very good driver.

Stirling Moss added to his growing reputation by winning the 500 c.c. race in no uncertain manner, and by his excellent drive in the main event to finish sixth in the H.W.M. No less a person than Tazio Nuvolari sought him out at the end of the race to express—through an interpreter—his admiration of Stirling's driving; no small compliment, that.

THE Italian G.P. was a terrific race, and the first occasion on which Ferrari have been able to challenge Alfa Romeo on level terms. As it was, they had only two cars against five Alfas, but three of the latter blew up (showing how hard they were pushed), while the sight of Ascari passing Farina to lead the race on the fourteenth lap was indeed something to bring the spectators to their feet. Later he took over Serafini's sister car after a long walk back to the pits when his own went out with axle trouble, and worked his way back to second place before the finish—a magnificent achievement. Farina's Alfa, the winner, appeared to have a little more power than the rest; Fangio blew up two cars, first his own and then Taruffi's, in his endeavours to hang on, while the remaining Alfa drivers were Fagioli, who finished third, and Sanesi.

The 1,100 c.c. race, before the Grand Prix, was notable for two near-dead heats; for first place, between the Oscas of Bonetto and Sommer (whose tragic death, recorded elsewhere, has cast such a shadow over the past week), and for third place between Simon's Simca Gordini and the Stanguellini (twin o.h.c. Fiat conversion) of Sighinolfi. Ken Wharton's Cooper 1,000 encountered a straw bale on the first lap, but Harry Schell lay sixth at half-distance until eliminated by a punctured carburettor float.

THE Formula 2 race at Mettet in Belgium on September 10 was run in two halves, the final classification being determined by adding together the competitors' times in each half, as at Albi. This resulted in a victory for Manzoni's Simca Gordini (second in both races) from the H.W.M.s of Stirling Moss (fourth in one, first in the other) and Lance Macklin (fifth in both). The works Ferraris of Ascari and Serafini each retired in one race, and consequently did not figure in the results.

COOPERS triumphed in the international 500 c.c. race at San Sebastian on September 3. They occupied the first six places in the final results. Again, these were decided by adding together competitors' times in the two separate events, and apparently only Ken Carter finished both races, winning on each occasion. He was therefore

COMING SHORTLY

- SEPTEMBER 23.—Cornwall Vintage C.C. Rally and driving tests, starting Berruthan Steps Café (Newquay-Padstow coast road), 3.30 p.m.
- 23.—Midland A.C. National speed hill-climb, Shelsley Walsh, near Worcester, 1.30 p.m.
- 24.—West Hants and Dorset C.C. Knott Challenge Cup and southern inter-club team trial, starting Bovington Camp, near Wareham, Dorset, 12.30 p.m.
- 24.—Bentley D.C. Firle Hill-climb, Bo-Peep Hill, Firle Estate (main Eastbourne-Lewes road), practising from 10 a.m.
- 24.—Southern Jovett C.C. Rally, at Crawford Farm, Balla Cross, near Petworth, Sussex, 12 noon.
- 24.—Pontedecimo Giovi hill-climb, Italy.
- 29.—Monte Carlo Rally British Competitors Club. Meeting open to all Monte Carlo Rally competitors. Fall Mall room, Royal Automobile Club, London, S.W.1., 7 p.m.
- 30.—B.A.R.C. Race meeting, Goodwood, 2 p.m.
- 30.—Scottish S.C.C. Closed hill-climb, Bo'ness.

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THE SPORT

awarded first place, the rest being classified as follows, according to the number of laps completed: 2, Jack Delhaes (of Belgium); 3, John Cooper; 4, Ted Frost; 5, Stan Coldham; 6, George Wicken.

BOTH the Czechoslovakia G.P. (September 24) and the Moroccan G.P. at Casablanca (November 12) have been cancelled.

THE attempt on long-distance class D international records (including the twenty-four-hour record) by a 2½-litre Aston Martin at Monthéry on September 5 was unsuccessful, fog bringing the proceedings to an abrupt conclusion. However, it is possible that another attempt will be made early in October.

PRESCOTT'S final meeting (see page 999) was excellent, Dennis Poore well deserving his success as well as the Hill-Climb Championship. Allard and Whar-ton (second and fourth fastest respec-

continued

tively) tied for second place in the championship, followed by Raymond Mays and Butterworth.

THOSE readers living in Yorkshire will be interested to hear of the formation of the British Amateur Motor Racing Club, which aims to enable members to compete in motor racing on a co-operative basis. Enthusiasts are invited to write to Ian A. Douglas, 15, Westfield Crescent, Riddlesden, Keighley, Yorkshire.

RACING at Goodwood, on September 30, should be excellent; among the Continental competitors will be Bira, de Grafenried, Johnny Claes and Harry Schell, while most of the British celebrities will compete, Reg Parnell with the B.R.M. (and good luck to it) as well as a Cooper. There will be three handicap races, a 500 c.c. race, a *formule libre* race, the Madgwick Cup (2,000 c.c. or 1,100 c.c. s) race, and the Goodwood Trophy (formula 1) race.

RAYMOND SOMMER

MOTOR racing suffered an irreparable loss on Sunday, September 10, when Raymond Sommer was killed in an accident during the G.P. de la Haute Garonne, in France. Driving John Cooper's 1,100 c.c. Cooper, he was leading the race by a big margin when he ran off the road, through hitting an oil patch while overtaking a slower car, and hit a tree.

Sommer, who was 44 years old, was one of the great personalities of motor racing. A wealthy amateur, he was always more interested in the enjoyment of motor racing—which was his whole life—than in the mere winning of the race itself. For this reason victory in the major events came his way but rarely; his policy was always to drive as fast as possible, and if the car broke down—well, that was just bad luck.

His first appearance in a race in this country was in 1935, when he drove his monoposto Alfa Romeo in the Donington G.P.; his last, of course, was on August 26 at Silverstone, at the wheel of a Cooper, an Aston Martin and the B.R.M. This year he was right on top of his form (in my own opinion, he was in the top rank of drivers, matched only



by Ascari and Fangio), but was never too busy or preoccupied to offer consolation and valuable advice to younger drivers, no matter how raw and unknown. Apart from his victory for Ferrari in the Prix de Berne, his most notable drives during the year were at the wheel of his own much-used Talbot; at Spa, where he seriously worried the official Alfa team, and indeed would have won had the car lasted; at Zandvoort (with a works Talbot), where he passed the Maseratis of Fangio and Gonzales to lead handsomely until almost half-distance; and at Monza, almost his last race, where he lay fourth for a long time, against the combined might of Alfa and Ferrari.

What use are words on an occasion like this? Everyone with the slightest interest in racing will miss Raymond Sommer: sportsman, humorist, volatile of temperament—and one of the nicest persons I have met.

J. A. C.

CLUB NEWS

Bentley D.C.—On Sunday, September 24, Bo-Peep hill on the Firle Estate (which is 10 miles from Eastbourne) will be the course for the Firle hill-climb. Bentley, Aston Martin, Frazer-Nash, Lagonda and Lancia cars will compete, and a special class will be run for members of the Brighton and Hove M.C. Length of the course is about 600 yards; average gradient, 1 in 7.75. Practice will start at 10 a.m.

West Hants and Dorset C.C.—Also this Sunday is the Knott Challenge Cup and Southern Inter-club team trial, with a sporting course taking competitors through Dorset from the start and to the finish at Bovingdon Camp, near Wareham. Members of the N.W. London, Bristol, Southsea, Harrow, Brighton and Hove, Sutton Coldfield, and Plymouth clubs have been invited to compete. Starting time will be 12.30 p.m. in this event.

Sheffield and Hallamshire M.C.—A crowd of 8,000 people watched the race meeting at Gamston Airfield on August 19, which included races for sports: 500 c.c. racing, M.G. and Ford-engined cars. Result: sports cars: up to 850 c.c.: 1, M.G. (Mrs. Pannell); 2, Austin (G. R. Gahagan). Up to 1,300 c.c.: 1, Cooper-M.G. (P. B. Reece); 2, M.G. (J. F. Dalton). Up to 1,800 c.c.: 1, Cooper-M.G. (P. B. Reece); 2, Riley

(Mrs. N. Binns). Up to 2,500 c.c.: 1, Frazer-Nash (E. J. Newton); 2, Cooper-M.G. (P. B. Reece). Unlimited: 1, Frazer-Nash (E. J. Newton); 2, Mercury (J. P. Chapman). M.G. cars handicap: 1, J. F. Dalton; 2, Mrs. Pannell. Ford-engined cars up to 1,175 c.c.: 1, A. M. Beardshaw; 2, R. D. G. Smith. Racing cars: 500 c.c.: 1, Cooper (E. R. Clarke); 2, Cooper (A. B. Rogers). 850 c.c. handicap: 1, Cooper (A. M. Beardshaw); 2, Cooper (A. B. Rogers). 1,500 s, 4,500 c.c.: 1, E.R.A. (T. C. Harrison); 2, Cooper (A. B. Rogers). Unlimited handicap: 1, Lagonda (H. Howorth); 2, M.G. (R. Sprackley).

Lancashire A.C.—Winner of the Davis Trophy trial, run on September 2, was G. P. Mosby, driving a Ford, with no marks lost. The opposite class award went to C. L. Bold (Bold Special), also with no marks lost. D. S. Rayner (Ford) was the only competitor in the touring group to finish with a clean sheet.

M.S. Car Club (S.W. Centre).—The Devon rally will be run on September 30, starting from the Seymour Hotel, Totnes, at 2.45 p.m., and finishing at the Seymour Hotel for tea, with a dinner and dance with cabaret to follow. All details from R. P. Dean, 186, Old Laira Road, Laira, Plymouth. Inclusive entry fee is 12s 6d.

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1949 Bristol, type 400 Saloon, as new	£1,550
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IRISH INTERNATIONALS

BLOWN CARS HAVE TROUBLE IN CURRAGH EVENT

THIS year both the Irish Motor Racing Club's international races at the Curragh, County Kildare, held on September 9, were handicap events, but the Wakefield Trophy Race (99 miles) was handicapped in such a way that the bulk of the supercharged cars started within a minute of one another, J. Kelly (Alfa two-stage) and K. Wharton (P. H. Bell's 2-litre E.R.A.) sharing the scratch mark, while the unsupercharged cars received a little over a minute's start. Each type was competing in a separate scratch class.

The blown cars retired with remarkable rapidity, A. G. Whitehead's E.R.A., G. N. Richardson's R.R.A., and J. G. Hillis's 6C Maserati being for all practical purposes out of the race after one lap, while Wharton made the first of several pit stops at the same time, though later at intervals he was lapping at 80 m.p.h.

A. Powys-Lybbe (monoposto Alfa Romeo) was fastest in the early stages, putting in a lap at 82.34 m.p.h., but Kelly beat that by one second at 82.73 and began to close up when Powys-Lybbe eased off after passing the unsupercharged group and taking the lead. Then the Alfa began to misbehave and retired on the 15th of the 20 laps with engine trouble, leaving Kelly with a good lead and the race in his pocket.

Gordon Watson (Alfa single-stage) went well for twelve laps but could not hold Duncan Hamilton (6C Maserati), who in his turn had lost his minute's start and another half-minute to Kelly by the sixteenth lap. Then Kelly's pit staff, whose signalling arrangements were

rather elementary, waved him down and he slowed from laps at 81, to 75 m.p.h. This dangerous easing allowed Hamilton to come up on him and the Maserati driver went all out, slipped by Kelly on a bend on the last lap and got across the finishing line three seconds ahead of him to win the race at 78.35 m.p.h. Kelly, however, had the consolation of winning his class and the prize for fastest finisher with an average of 79.35 m.p.h., a new record.

The O'Boyle Trophy race (74 miles) had a field of 33, half of whom finished, and last year's winner, J. J. Flynn, of Dublin (939 M.G. PB), off the limit mark, improved his average by 6 m.p.h. and won by half a minute from M. P. Cahill (M.G.-based 1,287 Iona Special), despite a much more severe handicap this year. J. Kelly (1,496 Riley-engined I.R.A.) made fastest lap at 72.3 m.p.h. and would certainly have been at least second but for a broken spring shackle.

RESULTS

O'Boyle Trophy Race (74 miles): 1, M.G. 939 (J. J. Flynn), 2 laps, 1m handicap, 1h 6m 2s, 61.68 m.p.h.; 2, Iona Sp 1,287 (M. P. Cahill), 2m handicap, 1h 8m 34s, 69.04 m.p.h.; 3, M.G. 1,250 (A. Coleman), 1 lap, 4m handicap, 1h 7m 2s, 62.37 m.p.h.; 4, M.G. 1,250 (T. N. Large), 5, M.G. 1,250 (C. Vard), 6, M.G. 1,250 (Miss E. Ingram). Fastest lap: I.R.A. 1,496 (J. Kelly), 4m 6s, 72.30 m.p.h.

Wakefield Trophy Race (99 miles): 1, Maserati 1,496 (D. Hamilton), 1m handicap, 1h 17m 40s, 78.35 m.p.h.; 2, Alfa 1,495 (J. Kelly), scr., 1h 17m 43s, 79.35 m.p.h.; 3, O.B.M. 1,971 (O. Moore), 1 lap, 1m handicap, 1h 18m 55s, 75.25 m.p.h.; 4, R.R.G. 1,674 (P. C. T. Clark), 1 lap, 2m handicap, 1h 19m 0s, 72.20 m.p.h.; 5, Healey 2,443 (R. F. Salvadori), 6, Maserati 2,640 (R. Dutt), 7, H.W. Alta 1,561 (T. M. Meyer). Fastest lap (record): Alfa (J. Kelly), 3m 35s, 82.73 m.p.h.

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IN BRIEF

Mr. C. D. Clayton, well known in the industry as "C. D.," died recently. He was advertising agent and press representative of all motor shows in this country up to the beginning of the last war.

Mr. Rowland G. Goodwin, manager of the Castrol motor sales department, is to retire after 42 years' service with C. C. Wakefield and Co., Ltd. He is succeeded by Mr. C. E. R. Millidge, who has been his assistant motor sales manager for some years.

A million dollars worth of orders for the Land Rover has been received by the manufacturers during a three-week period. In one day alone dollar orders worth more than £95,600 were placed. Although the Land Rover has been in production for only a little more than two years, its exports have earned more than five million pounds.

It is with great regret that *The Autocar* records the death of Mr. E. C. M. Burrell, who, as secretary of the Institute of British Carriage and Automobile Manufacturers, and general secretary of the National Federation of Vehicle Trades for the past 22 years.

The death is recorded, after a long illness, of Mr. George Victor Pettit on September 11, at Exeter. Born at Halifax, in 1889, George Pettit's career in the motor trade was a long one.

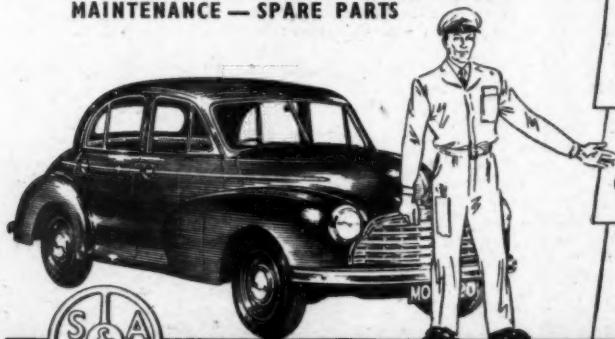
He ran a number of car and motor cycle depots known as Maudes Motor Mart at Norwich, Cardiff, Bristol, Walsall and London. In late years he had managed the City Garages, a service station and garage in Exeter. His car carried the registration Ar. The funeral was on September 15, at Mount Tabor Cemetery, Halifax.

Highways of Britain, a new book by L. H. Wilson, managing director of Wilsons Car Hire Services, Ltd., 34-38, Acre Lane, London, S.W.2, gives itineraries for motoring tours taking from one day to more than a month. Some special tours, such as "English Kings and Queens" are also included, and the whole book is well illustrated with photographs of most parts of the country. Published by Glen Freebairn, Lincoln Chambers, Portsmouth Street, London, W.C.2. Price 6s 6d (1 dollar).

Dunlop tyres were used by Stirling Moss in winning the 500 c.c. race in a Cooper-Norton, at 79.87 m.p.h. at the Silverstone International Trophy meeting on August 26. R. Sommer and A. J. Bottoms, who took second and third places respectively, also used Dunlops. In the 1,500 c.c. class of the Production Car Race, the first four places were taken by cars using Champion plugs. First was G. A. Ruddock's H.R.G., at 71.78 m.p.h., followed by the M.G.s of R. W. Jacobs, E. W. K. Lund, and G. E. Phillips.

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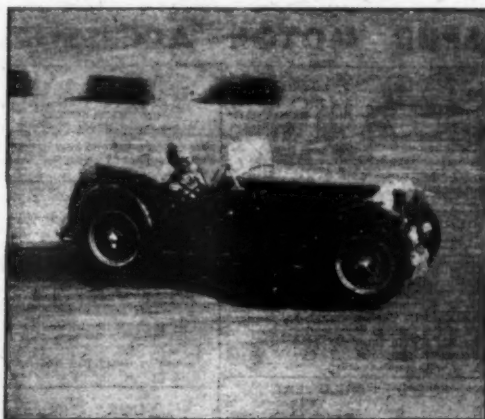
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M.G., Series T.C. (1947), 8,000 miles	35.462 m.p.h. : 34.42 m.p.g.	50.734 m.p.h. : 22.78 m.p.g.

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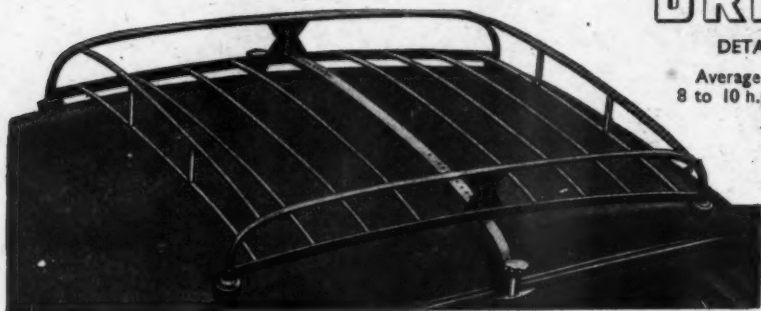
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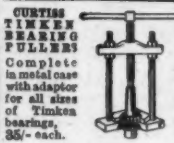
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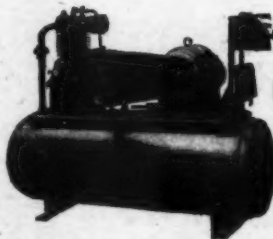
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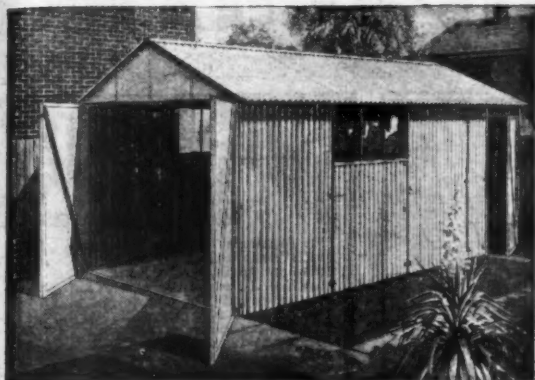
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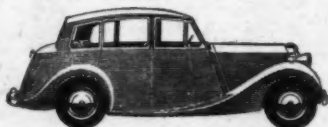
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1949 Alvis 14hp saloon, black, small mileage, choice of two; examined and approved by makers. [7136]

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ALVIS 1935 16.9 Silver Eagle sports saloon, clean condition, good tyres, D.W.S. permanent jacks; £260 or offer, write—K. Ayles, Ramet, Worcester, N. Wales. [7171]

£112—Specimen 1930 Silver Eagle (17hp) Alvis saloon, spec. 1st, 1,000 miles, 1,000 miles ago, receipts available, owner purchased Bugatti.—Box 6390. [7197]

1938 Alvis 17hp 4-door sports saloon, exceptional condition throughout, taxed, until 1951; £575.—The Workingham Motor Co., Ltd., Finchamstead Rd., Wokingham 380. [7089]

£260—Eagle 16 saloon, grey, 1935, body interior, tyres perfect, 600 miles since £80 overhaul, new battery, discs, Monotones.—Wogin, 5, West Common Gardens, Southampton, Lincs. 2357. [7681]

ALVIS Speed 20 sports saloon, managing director's car, recently completely overhauled at cost of £500, must be seen to be appreciated; £450.—St. Peter's Motors, Market Place, Chalfont St. Peter, Bucks. [7525]

GREY Alvis motor 25.6, 1929, first registered Jan. '41, laid up during war, total mileage 37,000, reconditioned throughout 1949, perfect order.—Apply Lord Somerleyton, Somerleyton, Suffolk. Tel. Blundstone 308. [6824]

1948 (August) Alvis 14hp saloon de Luxe, grey with leather upholstery and cloth seat covers, fitted radio and heater, maintained by Alvis throughout and just tuned by them, beautiful car; £1,125.—Box 6104. [7653]

ALVIS Speed 25 sports tourist, 1938-9 model, taxed A Dec. one owner, rebuilt by makers May 1949 at cost of £635, 2,500 miles only since rebuild, guaranteed perfect; £750.—Rowes Garage, Colwick, Nottingham. Tel. 52629. [7029]

£325 or near offer.—Alvis Speed 20, open 4-seater, just been reduced, rechromed, new hood, side curtains, carpets, tonneau cover, engine (triple S.U.S. coil and magneto ignition) runs excellently.—R. Shaw Ambassador 4501. [7026]

225 gns.—Alvis 1931 12/60 type special sports 2-seater, fitted 30hp Ford V.8 engine, British racing green, leather upholstery, concealed hood, very carefully used, exceptional condition, taxed; terms, exchanges.—Rowland Smith, below. [7026]

295 gns.—Alvis Firebird 1935 model 14hp 4-door sports saloon, black, 1st owner, green leather, Alfa discs, carefully used, very good condition; terms, exchanges; list: open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). [7919]

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ALVIS Crested Eagle, 20hp, 1935, recent £500 overhaul at Alvis works, in really good condition, owner just had delivery of new car; price £700, or offers.—Enquiries to be made to Transport Manager, John Fowler & Co., Limited, Leeds, Leeds, Leeds, Leeds. [7136]

1949 Alvis drop head, grey, recoloured, new hood fitted, complete overhaul at Alvis works, not used since, 13,000 miles and in beautiful condition; specimen condition throughout; £1,350; exchanges, deferred terms.—John S. Truscott, Ltd., 173, Westbourne Grove, W.11. Bay. 4274. [7422]

1949 (July) Alvis 14hp Tickford 4-seater drophead coupe, grey, fawn leather, 11,000 miles, Alvis maintained for one meticulously careful owner; specimen condition throughout; £1,350; exchanges, deferred terms.—John S. Truscott, Ltd., 173, Westbourne Grove, W.11. Bay. 4274. [7422]

1935 Alvis limousine, 7 seats, face forward, body by Mayfair Carriage Co., this car has not been used since September, 1939, fitted with discs, the original spare never been used, mileage 23,000, one owner; the car must be seen to be appreciated, it is absolutely as new.—A. Ringle, 262a, Clapham 1834. [3277]

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ALVIS 1935 16.9 Silver Eagle sports saloon, clean condition, good tyres, D.W.S. permanent jacks; £260 or offer, write—K. Ayles, Ramet, Worcester, N. Wales. [7171]

£112—Specimen 1930 Silver Eagle (17hp) Alvis saloon, spec. 1st, 1,000 miles, 1,000 miles ago, receipts available, owner purchased Bugatti.—Box 6390. [7197]

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225 gns.—Alvis 1931 12/60 type special sports 2-seater, fitted 30hp Ford V.8 engine, British racing green, leather upholstery, concealed hood, very carefully used, exceptional condition, taxed; terms, exchanges.—Rowland Smith, below. [7026]

295 gns.—Alvis Firebird 1935 model 14hp 4-door sports saloon, black, 1st owner, green leather, Alfa discs, carefully used, very good condition; terms, exchanges; list: open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). [7919]

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1949 Alvis drop head, grey, recoloured, new hood fitted, complete overhaul at Alvis works, not used since, 13,000 miles and in beautiful condition; specimen condition throughout; £1,350; exchanges, deferred terms.—John S. Truscott, Ltd., 173, Westbourne Grove, W.11. Bay. 4274. [7422]

1949 (July) Alvis 14hp Tickford 4-seater drophead coupe, grey, fawn leather, 11,000 miles, Alvis maintained for one meticulously careful owner; specimen condition throughout; £1,350; exchanges, deferred terms.—John S. Truscott, Ltd., 173, Westbourne Grove, W.11. Bay. 4274. [7422]

1935 Alvis limousine, 7 seats, face forward, body by Mayfair Carriage Co., this car has not been used since September, 1939, fitted with discs, the original spare never been used, mileage 23,000, one owner; the car must be seen to be appreciated, it is absolutely as new.—A. Ringle, 262a, Clapham 1834. [3277]

£445—Alvis 20hp 4-light saloon 1939, finished black and chrome with blue leather interior, whole car in original condition throughout; and a typical example of hand-built British coachcraft at its very best, mechanical order leaves little or nothing to be desired, general performance, braking, steering and road holding being just as one would wish to be, offered with a very comprehensive written guarantee by car; may be seen at Alvis works, 173, Westbourne Grove, W.11. Bay. 4274. [7422]

1937 (November) Alvis type T.A. 25/63 four-door, Charleworth saloon finished black with maroon hide upholstery, genuine total mileage 44,000, complete overhaul at works and new cylinder block fitted at £1,500 miles, almost new Ford Dunlop tyres, new car, in really excellent condition throughout, having been carefully maintained by fastidious owner; offers wanted.—Shaw Motors, Ltd., 666-678, Garratt Lane, London, S.W.17. Wim. 3031-2. [7401]

Genuine privately owned 16.95hp Alvis 4-seater 2-door four-seater d. 8 super sports coupe, brand new hood filelets and head linings, coach-built aluminium body and wings, colour original superior Alvis black, series car, low wind, low resistance, low weight, low understeering, low sweeping tail, this car is definitely immaculate inside and out and cannot be faulted; sold up 10 years, all tyre new 2 spare wheels, dual lemons, steering self-centering, brakes 100% efficient; a most attractive economical thoroughly reliable quiet running fast sports car; a thing of beauty is a joy for ever; 300 miles year, 300 miles year, 300 miles year; enquire; genuine bargain, £250; an honest car.—1 Bentley Rd., Manchester, 21. Tel. Chorlton 1288. [7055]

SILVER CREST 17hp sports saloon, unquestionably finest example of this model is superlative condition, equipped with many extra inclusions: P.100 large passenger swivelling spotlight, twin mellowtones, quarter rear bumpers, Runbaken oilclic, Rotoflo rear shock absorbers, chromed steering wheel, total mileage 60,000, engine and transmission completely reconditioned with all Alvis modifications at 46,000 including resleeving, new main bearings, clutch plate, etc., coachwork and interior recently completely reconditioned at cost of £225 by reputable firm coach repairers, body recoloured attractive metallic maroon, maroon hide upholstery, new headlining cloth and all interior woodwork repolished, exterior chrome repainted, any test will be given to genuine applicants; price £700 or near offer.—Tel. Royal 2631 daytime or Prospect 1405 evenings and week-ends. [7055]

ALVIS 1935 16.9 Silver Eagle sports saloon, clean condition, good tyres, D.W.S. permanent jacks; £260 or offer, write—K. Ayles, Ramet, Worcester, N. Wales. [7171]

£112—Specimen 1930 Silver Eagle (17hp) Alvis saloon, spec. 1st, 1,000 miles, 1,000 miles ago, receipts available, owner purchased Bugatti.—Box 6390. [7197]

1938 Alvis 17hp 4-door sports saloon, exceptional condition throughout, taxed, until 1951; £575.—The Workingham Motor Co., Ltd., Finchamstead Rd., Wokingham 380. [7089]

£260—Eagle 16 saloon, grey, 1935, body interior, tyres perfect, 600 miles since £80 overhaul, new battery, discs, Monotones.—Wogin, 5, West Common Gardens, Southampton, Lincs. 2357. [7681]

ALVIS Speed 20 sports saloon, managing director's car, recently completely overhauled at cost of £500, must be seen to be appreciated; £450.—St. Peter's Motors, Market Place, Chalfont St. Peter, Bucks. [7525]

GREY Alvis motor 25.6, 1929, first registered Jan. '41, laid up during war, total mileage 37,000, reconditioned throughout 1949, perfect order.—Apply Lord Somerleyton, Somerleyton, Suffolk. Tel. Blundstone 308. [6824]

1948 (August) Alvis 14hp saloon de Luxe, grey with leather upholstery and cloth seat covers, fitted radio and heater, maintained by Alvis throughout and just tuned by them, beautiful car; £1,125.—Box 6104. [7653]

ALVIS Speed 25 sports tourist, 1938-9 model, taxed A Dec. one owner, rebuilt by makers May 1949 at cost of £635, 2,500 miles only since rebuild, guaranteed perfect; £750.—Rowes Garage, Colwick, Nottingham. Tel. 52629. [7029]

£325 or near offer.—Alvis Speed 20, open 4-seater, just been reduced, rechromed, new hood, side curtains, carpets, tonneau cover, engine (triple S.U.S. coil and magneto ignition) runs excellently.—R. Shaw Ambassador 4501. [7026]

225 gns.—Alvis 1931 12/60 type special sports 2-seater, fitted 30hp Ford V.8 engine, British racing green, leather upholstery, concealed hood, very carefully used, exceptional condition, taxed; terms, exchanges.—Rowland Smith, below. [7026]

295 gns.—Alvis Firebird 1935 model 14hp 4-door sports saloon, black, 1st owner, green leather, Alfa discs, carefully used, very good condition; terms, exchanges; list: open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). [7919]

GATEHOUSE offer 1937 Austin 10 open road tourer, mechanically sound; £350.—**Gatehouse Motors**, Ltd., Highgate Village, London, N.6. Mountview 4444.

1939 Austin 10 saloon, black, in excellent condition, throughout, any trial, bargain; £395.—**Wembley Court Motors**, High Rd., Wembley; Arnold 5221-2.

1947 late Austin 10, 15,000 miles only, radio, three months' guarantee; £685.—**Hendon Central Garage**, Ltd., 44-46, Watford Way, Hendon Central N.W.4. Tel. Hendon 8084-5-6.

1946 Austin 10 de luxe saloon, black with chrome leather, low mileage, 3-month guarantee, choice of two—**Trinity Cars**, Ltd., 94, North Side, Wandsworth Common, S.W.18. Battersea 1166.

1938 (May) Austin 10 Cambridge de luxe saloon, original cellulose, really excellent throughout; £375; another, 1937, taxed; £345.—**Harry Nash Motors**, 540, King St., Hammersmith, Riverside 2837-5.

1937 (late) 4 doors, sliding roof, special Bedford cord interior, extremely clean, practically new, chrome, cellulose excellent, 6,000 miles since overhaul, good tyres, exceptional car; £345.—**Collins**, Richmond 2536.

1937 Austin 10 Cambridge saloon No. G/RL/115592, genuine mileage 34,000, new tyres, taxed, Dec., first-class condition mechanically and bodywork, a family since new; £375.—A. Halewood, Woodside, Oakwood Rd., Cardiff.

1939 Austin 10hp Cambridge saloon de luxe, complete, 1937, in first-class mechanical condition, late property of London professional gentleman, sole reason for sale necessity purchase a larger bodied vehicle, genuine opportunity at the price.

CLAMEND MOTORS, Lake St., Leighton Buzzard, Beds. C. Tel. 2041-2-3. Write for our 16-page post free catalogue of at once 400 fully guaranteed used cars, and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive.

Austin Ten Cars Wanted

C THE CAR MART, Ltd., London distributors, wish to purchase Austin 10 cars.—297, Euston Rd., N.W.1. Euston 1212.

1939 Austin 10hp post-war Austin 10 urgently.—50, Ryecroft Rd. S.W.16. Tulse Hill 1288.

1939 Austin 10 wanted, not earlier than 1935.—**Gordon Wooderson**, 46a, Drewstead Rd., S.W.16. Streatham 3558.

ROWLAND SMITH'S, the Austin 10 buyers.—Hampstead High St. (Hampstead Tube). Hampstead 6041.

CASH buyers of low mileage Austin 10s; distance no object.—**Hattens**, Lord St., Southport. Tel. 2268.

1939—1949 Austin 10 saloon urgently required.—**Richard France**, Ltd., 254, High Rd., Tottenham, N.15. Sca. 2232 and 0464.

W must be in perfect condition.—**Princoff's Garage**, Minehead, Somerset. Tel. Minehead 495.

1938-9 Austin 10hp Cambridge saloon, in good condition.—Full particulars please to **Batty & Sons**, Ltd., Elin St., Sheffield, 1. Tel. 25547.

RAYMOND WAY, the hire-purchase specialists, are still buying pre-war Austin 10s and are unlimited cash available.—**Canterbury Rd., Kilburn, N.W.6. Maids Vale 6044** (10 lines).

AUSTIN A40

HAROLD PERRY, Ltd., Invicta Works, 279, Ballards Lane, North Finchley, N.12. Tel. Hillside 4444.

1948 Austin A40 saloon, first registered December, with heater, mileage 11,000, car is in perfect condition; £695.

HAROLD PERRY, Ltd., Invicta Works, 279, Ballards Lane, North Finchley, N.12. Tel. Hillside 4444.

CAR MART, LTD.

LONDON DISTRIBUTORS.

AUSTIN A40 1949 Devon saloon, radio and heater, 8,000 miles; £635.—**Car Mart**, Ltd., 297, Euston Rd., N.W.1. Euston 1212.

H. A. SAUNDERS, Ltd., offer:—

1949 Austin A40 saloon, black with blue upholstery, heater, 8,000 miles; £695.

H. North Finchley, (100 yds north of Tally Ho Corner), Hillside 0024.

COLINDALE SERVICE STATION offer:

1949 Austin A40 Dorset, very low mileage, one owner, heater, as new; £875.—**Colindale Service Station**, 155, 157 and 159, Edgware Rd., N.W.9. Tel. Colindale 6122.

WADDINGTON MOTORS, Ltd., offer:—

1948 (October) Austin A40 Dorset saloon, fitted with H.M.V. radio and heater, taxed year, excellent condition; £795.—**Fortune Green Rd.**, N.W.6. Man. 2211.

1948 Austin A40 Dorset saloon, mist green, low mileage, recommended; £825.

PERRARIS OF CRICKLEWOOD, Ltd., 200-220, Cricklewood Broadway, N.W.2. Gs. 2234.

1949 Austin A40 Devon saloon, 1,500 miles, N.W.1. Euston 4466.

1949 Devon saloon, grey, heater, small mileage; £865.—**Austin House**, 144, Golders Green Rd., N.W.11. Speedwell 0011.

1948 Austin Devon saloon, mist green, low mileage, heater; £825.—**Pantiles Service Garage**, London Rd., Guildford 5326.

1949 Austin A40 4-door sun saloon, heater, 10,000 miles, mist green, 1949, supplied.—**Ally & Bernard Ltd.**, 372, King St., S.W.3. Fla. 7245.

NAYLOR & ROSE, Ltd.—1949 Austin A40 de luxe saloon, beige, unblemished condition, radio and heater, low mileage, any trial; £695; three months' guarantee; choice of 250 quality cars; demonstrations free within 100 miles; terms available.—25, East Hill, Clapham Junction, S.W.18. Bick. 5272. Open 9-6 every week-day including Saturday.

Austin A40 Cars Wanted

D.C.S.

THE Austin A40 buyers.

THE CAR MART, LTD.

AUSTIN cars

REQUIRED immediately.

MAKE your enquiries to

AUSTIN House, 297, Euston Rd., London, N.W.1.

ROAD, London, N.W.1.

TELEPHONE: Euston 1212.

WE are requiring A40 Devons.—**Brown & Mallinson**, Ltd., Blackpool. Tel. 23322.

ROWLAND SMITH'S, the Austin A40 buyers.—Hampstead High St. (Hampstead Tube). Ham. 6041.

SELL your A40 to us, all Austin models urgently wanted.—**Osford**, 67, George St., W.1. Wel. 5393.

CASH buyers of low mileage Austin A40s; distance no object.—**Hattens**, Lord St., Southport. Tel. 2268.

URGENTLY required, Austin A40, Covenant free, low mileage, no dealers.—**Robinson**, 2, Regent St., Cambridge.

AUSTIN A40 cars wanted—**Motorists** (London).

A. Ltd., are immediate cash buyers of A40s and 10hp saloons.—**Great North Rd.**, E. Finchley Station, N.2. Tudor 2301-2.

AUSTIN TWELVE

CAR MART, LTD.

LONDON DISTRIBUTORS

AUSTIN 12 1947 saloon, 6 months' guarantee; £850.—**Car Mart**, Ltd., 297, Euston Rd., N.W.1. Euston 1212.

1939 Austin 12 saloon, excellent condition throughout, taxed; £375.

PARSONS & PARSONS (GARAGES), Ltd., Potter St., Harlow, Essex. Tel. 121.

1939 Austin 12/4 de luxe saloon, one private owner, clean condition.—**Below**.

1926—Austin 12 special series Greyhound sports saloon, in exceptional condition throughout, out, bargain.—**Bray Motors**, 180-184, West End Lane, N.W.6. Hampstead 6490.

ARCHIE SIMONS & Co., Ltd.—1939 Austin 12hp saloon; £425.—94, St. Portland St., W.1. 140.

AUSTIN 12 saloon (grey) April '49, only 4,000 miles, almost new condition; £900.—**Perry**, BM/BPMA, London, W.C.

1939 Austin 12 saloon, excellent mechanically; £765.—**Smith & Hunter**, 376, Kensington High St., W.14. Western 2312.

1937—Austin heavy 12-4 tourer, in exceptional order, a car for excellent service and reliability. See **Cricklewood** 57.

1925 Austin 12hp saloon de luxe, 1947 model, finished black with brown leather, nominal mileage, tip-top mechanical order.

C. Tel. 2041/2/3. Write for our 16-page post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire-purchase facilities; part-exchanges; free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive.

DECEMBER 1933 12hp saloon, maroon/brown, low mileage, excellent condition for year; £215.—**Austin House**, 144, Golders Green Rd., N.W.11. Speedwell 0011.

1926—Austin 12-4 de luxe saloon, in original immaculate condition, one owner, Ray Clean when last used, 25-10-35, believed 1936 model.—**Hampstead 4593.**

1937 (August) Austin 12 saloon, just had complete engine overhaul, excellent condition for year; £215.—**Austin House**, 144, Golders Green Rd., N.W.11. Speedwell 0011.

1940 Austin 12 saloon, black, brown, excellent condition, identical to post-war model; £550.—**Peter Bantock Car Sales**, 104, High Rd., Chiswick, W.4. Chiswick 2725/5870.

Austin Twelve Cars Wanted

C THE CAR MART, Ltd., London distributors, wish to purchase Austin 12 cars.—297, Euston Rd., N.W.1. Euston 1212.

CASH buyers of low mileage Austin 12s; distance no object.—**Hattens**, Lord St., Southport. Tel. 2268.

WANTED, 1947-8 Austin 12hp saloon, no dealers.—**J. Scales**, 426, Cherrylinton Rd., Cambridge.

ROWLAND SMITH'S, the Austin 12 buyers.—Hampstead High St. (Hampstead Tube). Hampstead 6041.

1937-9 Austin 12 saloons and limousines wanted.—**Motorists** (London), Ltd., East Finchley Station, N.2. Tudor 2301-2.

RAYMOND WAY, the hire-purchase specialists, are still buying pre-war Austin 12s and have unlimited cash available.—**Canterbury Rd., Kilburn, N.W.6. Maids Vale 6044** (10 lines).

AUSTIN SIXTEEN

CAR MART, LTD.

LONDON DISTRIBUTORS.

AUSTIN 16 1949 saloon, 12,000 miles; £965.—**Car Mart**, Ltd., 297, Euston Rd., N.W.1. Euston 1212.

CARR BROS offer:—

1932 Austin 16hp, a one owner car in remarkable condition, 32,000 miles only and just been fitted with a reconditioned engine, original cellulose and carpets, a specimen car owned by a most fastidious owner; only offered for sale by delivery of new car; £225.

CARR BROS. GARAGES, Parley, Uplands 4811-2-3. Open 9-6 every day.

DICKS CAR SALES offer:—

1946 (December) Austin 16 saloon, one owner, really terrific, 16,000 miles, immaculate condition; £625.

DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maids Vale 6988-9.

D. J. SHEPHERD & Co. (Barnfield), Ltd., offer:—

1948 (September) Austin 16hp, black, brown interior, 16,000 miles, immaculate condition, taxed and of year; £875.—**D. J. Shepherd & Co.** (Barnfield), Ltd., 436, Hertford Rd., Barnfield, Howard 1631.

1946 Austin 16 saloon, dark blue, excellent condition, taxed; £625.

PARSONS & PARSONS (GARAGES), Ltd., Potter St., Harlow, Essex. Tel. 121.

1948 Austin 16 for sale, 15,000 miles, one owner; offer over £825; view St. Albans or Watford. Box 641d.

1947 Austin 16hp saloon de luxe, black/brown leather, radio and heater, one owner, guaranteed; £925.

G. W. WILKIN, Ltd., 1, Weston Park, Kingston-on-Thames, Kin. 2241.

1947 Austin 16, low mileage, one owner, excellent condition; £855; reconditioned black, no dealers.—**Box 6197.**

1949 Austin 16 Countryman, 12,000 miles.—**British & Colonial Motors**, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 5583.

GORDON CARS (LONDON), Ltd.—1948 Austin 16hp, green, 10,000 miles.—**Gordon House**, 373, Euston Rd., N.W.1. Euston 6611.

1947 Austin 16, low mileage, one owner, excellent condition; £895.—**Bowen**, Hillside Garage, Edgware 4464-5.

1937 Austin 16 de luxe all black, black leather upholstery, reconditioned engine, in immaculate condition throughout.

MAKIN & HARRISON (MOTORS), 492-6, High Rd., Chiswick, Chiswick 0556/2619.

1948 Austin 16 saloon, Fordland grey with brown leather upholstery, one owner, 14,000 miles, indistinguishable from new; £925.

HUNGERFORD MOTORS, Ltd., 201-3, Upper Richmond Rd., S.W.15. Putney 0222 and 3360.

1948 (September) Austin 16hp saloon, black, brown leather, genuine mileage 12,000 only, end of year perfect condition; £890.—**R. S. Mead**, 42, Queen St., Maldenhead, Tel. Maldenhead 2642.

1947 16hp saloon, black/brown hide upholstery, low mileage, heater, exceptional condition; £875.—**Austin House**, 144, Golders Green Rd., N.W.11. Speedwell 0011.

POST-WAR Austin 16, almost unmarked inside and out, speedometer reading 10,000 odd; accept £795.—**Jack Rose**, Ltd., Stafford Rd., Wallington, Surrey. Wallington 6677-8.

1946 (June) Austin 16, in very clean condition, black with leather upholstery all through.—**Shaw Motors**, Ltd., 666-678, Garratt Lane, London, S.W.17. Tel. 531-3.

WALTER SCOTT, Ltd.—1946 (Nov.) Austin 16 saloon, black, brown hide, low mileage, excellent condition; £875; terms, exchanges.—39, College Crescent, Hampstead, N.W.3 (Swiss Cottage Tube). Fri. 5914.

1946 (June) Austin 16, in very clean condition, black with leather upholstery all through.—**Hampstead, N.W.3. (Swiss Cottage Tube).** Primrose 5914.

1947 (Aug.) Austin 16, black, brown leather, cellulose covers since new, twin spot lamps, very carefully used and serviced—exceptional car; £825 or near offer.—**Markham**, 82, Victoria St., S.W.1. Tel. 7631.

1949 (April) Austin 16, 2-tone green, 10,000 miles, all leather; £945.—**Aeres Autos**, 10 and 11, Ascot Parade, Clapham Park Rd., S.W.4. 2 minutes from North Underground. Tel. Macaulay 5762 and Mal. 2873.

1948 Austin 16, black, brown leather, genuine 16,000 miles, all leather; £845.—**Ascot Autos**, 10 and 11, Ascot Parade, Clapham Park Rd., S.W.4. 2 minutes from Clapham North Underground. Tel. Macaulay 5762 and Mal. 2873.

1948 (November) Austin 16hp saloon, black, fitted with radio in perfect condition, one owner; £850 or nearest offer.—**Apply in first instance to North Bridge Service Depot, Ltd., Uxbridge Rd., Southall, Tel. Southall 2355**, who will demonstrate on owner's behalf.

Austin Sixteen Cars Wanted

T THE CAR MART, Ltd., London distributors, wish to purchase Austin 16 cars.—297, Euston Rd., N.W.1. Euston 1212.

CASH buyers of low mileage Austin 16s; distance no object.—**Hattens**, Lord St., Southport. Tel. 2268.

WANTED, 1947-8 Austin 16hp saloon, no dealers.—**J. Scales**, 426, Cherrylinton Rd., Cambridge.

1949 Austin 16hp saloon, black, brown leather, radio and heater, one owner, 14,000 miles, indistinguishable from new; £925.

G. W. WILKIN, Ltd., 1, Weston Park, Kingston-on-Thames, Kin. 2241.

1947 Austin 16, low mileage, one owner, excellent condition; £855; reconditioned black, no dealers.—**Box 6197.**

1949 Austin 16 Countryman, 12,000 miles.—**British & Colonial Motors**, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 5583.

GORDON CARS (LONDON), Ltd.—1948 Austin 16hp, green, 10,000 miles.—**Gordon House**, 373, Euston Rd., N.W.1. Euston 6611.

1947 Austin 16, low mileage, one owner, excellent condition; £895.—**Bowen**, Hillside Garage, Edgware 4464-5.

1937 Austin 16 de luxe all black, black leather upholstery, reconditioned engine, in immaculate condition throughout.

MAKIN & HARRISON (MOTORS), 492-6, High Rd., Chiswick, Chiswick 0556/2619.

1948 Austin 16 saloon, Fordland grey with brown leather upholstery, one owner, 14,000 miles, indistinguishable from new; £925.

HUNGERFORD MOTORS, Ltd., 201-3, Upper Richmond Rd., S.W.15. Putney 0222 and 3360.

1948 (September) Austin 16hp saloon, black, brown leather, genuine mileage 12,000 only, end of year perfect condition; £890.—**R. S. Mead**, 42, Queen St., Maldenhead, Tel. Maldenhead 2642.

1947 16hp saloon, black/brown hide upholstery, low mileage, heater, exceptional condition; £875.—**Austin House**, 144, Golders Green Rd., N.W.11. Speedwell 0011.

H. A. SAUNDERS, Ltd., offer:—

1949 Austin A70 Hampshire saloon, grey with grey leather and cloth upholstery, 3,000 miles; £1,195.

H. A. SAUNDERS, Ltd., Austin House, High Rd., North Finchley, (100 yds north of Tally Corner), Hildeside 0024. (7677)

PHILIP RICKARDS, Ltd., offer:—

1949 Austin A70, green, 6,000 miles, radio, seat covers, perfect.—4, Brick St., Park Lane, W.1. Grosvenor 4772/3. (7293)

WARWICK WRIGHT, Ltd., offer:—

1949 Austin A70 Hampshire saloon, black, brown leather and cloth, 9,000 miles; £1,275. W.1. Grosvenor 4772/3. (7293)

WARWICK WRIGHT, Ltd., 150, New Bond St., W.1. Mayfair 9761. (7615)

Mc KINNON MOTORS, Ltd., offer:—

1949 (July) Austin A70 Hampshire saloon de luxe, black, brown hide front seats, brown hide cloth rear, radio, built-in, heater, sliding roof, one owner, mileage 4,500; £1,245; write for complete list of post-war used cars in stock; trade enquiries welcomed.

Mc KINNON'S, Langham House, 3, Stafford Rd., Wallington, near Croydon, Surrey. Established 1906. Tel. Wallington 3404. (7433)

S. G. SMITH (MOTORS), Ltd., offer:—

1949 Austin A70, radio and heater; £1,125.—G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New Cross 4444. (5614)

CHARLES RICKARDS, Ltd., the house of standing and repute.

1949 (June) Austin A70 Hampshire saloon, one owner, 5,000 miles, in every respect absolutely as new and is being offered at a competitive price. Bayswater Rd., W.2 (next door to Lancaster Gate Tube Station). Tel. Paddington 1320, 17502

1949 Austin A70 saloon, grey, grey leather throughout, 2,100 miles, genuine, indistinguishable from new.—Ripco, Ltd., 16, Albemarle St., W.1. Regent 222. (784)

AUSTIN A70 1949 (September), grey, grey hide throughout, roadlamp, one owner, 10,000 miles only, exceptionally well maintained; £1,185; small saloon exchange, cash and part-exchange. Tel. Western 1226. (7816)

Austin A70 and A90 Cars Wanted

URGENTLY required, Austin A70, Coventry free, low mileage, no dealers.—Robinson, 2, Regent St., Cambridge. (6783)

PRIVATE advertiser requires A90 Atlantic, preferably power operated hood; full details.—1, Wellington Street East, Salford, 7, Lancs. (6811)

AUSTIN EIGHTEEN

CAR MART, LTD.

LONDON DISTRIBUTORS.

AUSTIN 18 1936 York long chassis saloon; £615.—Car Mart Ltd., 297, Euston Rd., N.W.1. Euston 1212.

1937 7-passenger saloon, one private owner, immaculate blue, blue leather.

1937 7-passenger limousine, division, forward seats, one private owner only, recorded mileage, black leather throughout, superlative mechanical condition; convenient hire purchase terms. C. Mortlake, 253, Kensal Rd., W.10. Ledbrooke 3155. (8007)

£475—Magnificent Austin 18 7-seater limousine, 1937 modification, easy-clean wheels, Girling brakes, bodywork literally brand new, excellent mechanically.

LAMBS OF WOOD GREEN, Finchley Showrooms, 421, High Road, N.12. Finchley 6221. (7803)

£675—1938, full 7-seater, nicely finished in black and chrome and upholstered in dark blue leather throughout, all in exceptionally clean condition, fully equipped with face-forward occasional seats and division, generous luggage space in external rear boot, spot-lamps and other extras; especially recommended mechanically, extensive servicing carried out by Austin distributors in May this year, tyres and battery renewed last month; whole car in the condition to give a lasting period of reliable service and fully guaranteed in writing by

CAMDEN MOTORS, Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Sixteen other Austin 7-seaters in stock, also 350 cars of other makes. Write for our 18-page post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire-purchase facilities; part-exchanges; free delivery; showrooms open till 8 p.m., Mon.-Sat. inclusive. (7854)

A & S Offer selected quality carriages—(not cheap and doubtful variety) inspection invited:—

SEVEN SEATERS 1937/1938/1939 Saloons and partitioned Limousines, excellent condition; leather throughout, black certified mechanically. Seen:—

L & S & SAUNDERS (100 Limousines: Lists posted). Providence Court, Grosvenor Square, 2941-Mayfair.

Austin Eighteen Cars Wanted

CAMDEN MOTORS, Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Sixteen other Austin 7-seaters in stock, also 350 cars of other makes; write for our 18-page post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part-exchanges; free delivery; showrooms open till 8 p.m., Mon.-Sat. inclusive. (7854)

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CAMDEN MOTORS, Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Sixteen other Austin 7-seaters in stock, also 350 cars of other makes; write for our 18-page post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part-exchanges; free delivery; showrooms open till 8 p.m., Mon.-Sat. inclusive. (7854)

A & S Offer selected quality carriages—(not cheap and doubtful variety) inspection invited:—

SEVEN SEATERS 1937/1938/1939 Saloons and partitioned Limousines, excellent condition; leather throughout, black certified mechanically. Seen:—

L & S & SAUNDERS (100 Limousines: Lists posted). Providence Court, Grosvenor Square, 2941-Mayfair.

Austin Twenty Cars Wanted

ROWLAND SMITH'S, the Austin 20 buyers.—Hamstead High St. (Hamstead Tube). Hamstead 6041. (0923)

AUSTIN TWENTY-FOUR

1932 Austin 7-seater with division, very clean throughout, excellent mechanical condition: £130.

FERRARIS OF CRICKLEWOOD, Ltd., 200-220, Cricklewood Broadway, N.W.2. Cica. 2234. (7963)

1938-9 Austin 24hp 7-seater limousine, low miles, private, one owner. Write BCM/NZA, London, W.C.1. (7625)

FOR SALE—Austin 23hp 1936 limousine in excellent condition throughout in blue and black; £425.—Vincent Garage, Glasbury, Somerset. (7112)

AUSTIN A125 and A135

WARWICK WRIGHT, Ltd., offer:—Austin Sheerline saloon, black, brown leather, 7,000 miles; £1,755.

WARWICK WRIGHT, Ltd., 150, New Bond St., W.1. Mayfair 9761. (7614)

PHILIP RICKARDS, Ltd., offer:—

1949 Austin Sheerline, black/beige, 12,000 miles, one owner; as new.—4, Brick St., Park Lane, W.1. Grosvenor 4772/3. (7292)

1949 Austin Princess, 6,000 miles, silver, unmarked; £1,875 or exchange shoeing brake and car.—Alaska, Court Mount, Canterbury Rd., Bournemouth. (7176)

1949 Austin Sheerline black, 10,000 miles, unmarked; £1,875 or exchange shoeing brake and car.—Alaska, Court Mount, Canterbury Rd., Bournemouth. (7176)

AUSTIN Princess saloon, March 1949, in peach with grey with beige leather, genuine 14,000 miles only, 11,000 miles, a superb car in perfect condition; £1,875.—Apply to H. C. Baigent, Northfields House, Wandsworth Park, S.W.18. Tel. Vandyke 4482. (6913)

Austin A125 and A135 Cars Wanted

C **THE CAR MART**, Ltd., London distributors, wish to purchase Austin Sheerline and Princess cars.—297, Euston Rd., N.W.1. Euston 1212. (0952)

CYRIL SHEPHERD requires:—Austin Sheerline.—102, King's Rd., Reading 2712. (7999)

AUSTIN MISCELLANEOUS

REGIONAL DISTRIBUTORS.

HIRE car and limousine specialists.

WRITE for details and location of cars to:

SALES Dept.: 45, South Audley St., London, W.1

TANKARD & SMITH, Ltd., offer 1949 Austin A70 saloon in blue with beige leather, genuine 9,000 miles only, in every way equal to new, fitted radio and heater; £1,195.—Below.

TANKARD & SMITH, Ltd., offer 1947 Austin 12 saloon in black with brown leather, moderate mileage only, unmarked in appearance; £825.—Below.

TANKARD & SMITH, Ltd., offer 1949 A40 saloon in black with brown leather, moderate mileage only, very attractive car, sun roof; £875.—Below.

TANKARD & SMITH, Ltd., offer 1946 Austin 16 saloon in black with brown leather, moderate mileage only, heater and radio, any trial; £775.—Below.

TANKARD & SMITH, Ltd., offer 1940 Austin 8 4-door saloon in black with brown leather upholstery, very nice condition, good tyres; £410.—Below.

TANKARD & SMITH, Ltd., offer 1939 Austin 10 saloon in black with green leather, good mechanical condition, very attractive car; £425.—Below.

TANKARD & SMITH, Ltd., offer 1939 Austin 12 saloon in black with brown leather upholstery, coachwork and interior very clean, mechanically first class; £450.—198, Kings Rd., S.W.3. Tel. Flaxman 4801/2/3. (7268)

TANKARD & SMITH, Ltd., offer the choice of many Austin 8s, 10s 12s and higher horse-power limousines from their vast stock of over 200 used cars, all subject to three months' written guarantee.—198, Kings Rd., S.W.3. Tel. Flaxman 4801/2/3. (0374)

Austin Miscellaneous Cars Wanted

ROWLAND SMITH'S, the Austin buyers.—Hamstead High St. (Hamstead Tube). Ham. 6041. (0926)

C. G. NORMAN & Co.

AUTHORIZED Austin dealers, are buyers of low-mileage Austin cars of any horse-power.—46-52, Vauxhall Bridge Rd., London, S.W.1. Victoria 7611-6.

AUSTIN wanted.—Smith's, 86, Chalk Farm Rd., N.W.1. Gul. 2767. (0922)

CASH immediately for good Austin.—H. F. Edwards, 154, Gt. Titchfield St., W.1. Langham 0012. (7767)

TOBBITT & TAYLOR urgently require all 1936 Austin.—22, Conduit Works, W.1. Amb. 6049. (4215)

RAWLINGS Bros., Ltd., 87a, Cromwell Rd., S.W.7. Tel. Froisher 8161 are buyers, all types post-war Austins. (0416)

BRITISH & COLONIAL MOTORS, Ltd., require good Austin cars.—Upper St. Martin's Lane; W.C.2. Tel. 3586. (7470)

ALLWEATHER MOTOR CO., High St., Edgware, require 50 Austins; 1935s to 1949s urgently.—Tel. Edgware 3510. (7086)

WEYBIDGE AUTOMOBILES, Ltd., the Austins (7086) Webbridge 235

WARRIOR urgently require late type Austins. Tel. Weybridge 235. (0541)

MOODALEY MOTOR CO., good buyers of Austin cars. 31, Trinity Rd., Wandsworth Common, S.W.18. Battersea 5573. (7391)

CHARLES RICKARDS, Ltd., wish to purchase good post-war Austin cars.—56, Bayswater Rd., W.2. Paddington 1820. (7307)

JACK OLDING, Ltd., 8-10, North Audley St., W.1. Austin retailers, require cars in first-class condition. Mayfair 5242. (0812)

AUSTIN heavy 16 or heavy 12 wanted, 1934 would do but must be really clean and sound.—S. Thompson, Rhinoceros, 10, St. John's St., E.C.4. (7682)

APPROACH us first before disposing of your Austin car.—Tankard & Smith, Ltd., 194/198, Kings Rd., S.W.3. Flaxman 4801/2/3. (6293)

WE are cash buyers of all Austin post-war models. Write for details.—The Warren Motor Co., 353-355, Euston Rd., London, N.W.1. Euston 7751. (6882)

1938 and genuine mileage stated.—Wilson, 16, Trinity Clns., S.W.9. Wrixton 4011. (0513)

AUSTIN 12 or 16, 1947 onwards urgently required to be sold from large public company.—L. T. W. Carter, 73, Cadogan Lane, S.W.1. Tel. Sloa. 4727. (0513)

Austin Spares and Service

NORMAND, Ltd.

The best service ensures a longer car life.

BRING your car to 405-9, King St. W.6. Riv. 3565. (0328)

THE CAR MART, Ltd.

LONDON distributors, spare parts for all models, cars and trucks.

THE CAR MART, Ltd., Welsh Harp, Edgware Rd., N.W.9. (Hendon 6500); and at 16, Uxbridge Rd., Ealing, W.5. (Ealing 6117). (0146)

C. G. NORMAN & Co.

AUTHORIZED Austin dealers.

SPARE parts and components in stock.—46-52, Vauxhall Bridge Rd., London, S.W.1. Victoria 7611-6.

IF it's Austin spares, try Sands, the Austin People. Burnham, Bucks 84. (0302)

BARKERS MOTORS (LONDON), Ltd., Tel. Balham 6666 for Austin spares, sales and service.—200, Balham High Rd., S.W.17. (0504)

AUSTIN 7 spares, 1923-33, practically everything in stock, new and second-hand.—Price, Three Shires Oak, Bearwood, Birmingham. (1866)

AUSTIN 7 spares, any year, any part, largest stockists in U.K.; exchange units.—Try Northwood's firm, 45-47, Newmarket Causeway, S.E.1. Hoy 2838, 2820. (0266)

BROCKHURST GARAGE—Harrow agents for Austin sales, service, spares, reconditioned units.—Uxbridge Rd., Harrow Weald, Middlesex. Tel. Grimsdyke 561. (0203)

AUSTIN 7 owners—Take advantage of our comprehensive stock of spare parts and replacement units; price list on application.—Fairley's, Austin Seven specialists, John St., Sheffield, 2. Tel. 23876. (0286)

WITHAM'S offer replacement parts for Austin 7 at very keen prices; everything in stock; exchange units; write us first and save time and money.—Witham's Motors, Ltd., 18, Balham Hill, S.W.12. Battersea 3280/3769. (0615)

AUSTIN spares.—Largest stockists in South London for Austin car and commercial vehicle parts; new and replacement units from stock; free delivery many areas.—Wimbledon Motor Works, Ltd., 29, High St., S.W.19. Wilm. 0123. (0414)

PRYNN & STEVENS, Ltd., the South London Austin exchange, claim range of parts and units in stock; exchange engines, gear boxes, pumps, clutches, carburetors brake shoes and electrical units from stock; repairs and service to Austins exclusively.—57, Acree Lane, S.W.2. Britton 155. (0184)

BENTLEY (3½ & 4½-litre)

P

B

LTD.

1935 3½-litre Bentley Park Ward sports saloon.

1934 3½-litre Bentley Mann Egerton sports saloon.

PADDON BROS., Ltd., 60, Cheval Place, South Kensington, London, S.W.7. Ken. 9477/8. (7608)

HOFFMANN'S GARAGE, Ltd.

HUDDESFIELD RD.

HALIFAX.

GREAT Britain's leading specialists in Rolls-Royce and Bentley cars.

PROUD members of the Swain group.

A NATIONAL motoring organisation.

1939 Bentley 4½-litre overdrive saloon by Park Ward. Ref. H.5026.

1937 Bentley 4½-litre saloon by H. J. Mulliner. Ref. H.4767.

1936 Bentley 4½-litre semi razor edged sports saloon by Thrupp & Maberly. Ref. H.4387.

1936 Bentley 4½-litre sports saloon by William Arnold. Ref. H.5011.

ALL cars carry out unique six months' guarantee; please write or phone for details to:—

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HALIFAX.

TEL. Halifax 5944.

FOX

OFFICIAL retailers of Rolls-Royce chassis; official retailers of Bentley chassis; appointed retailers of H. J. Mulliner coachwork, we are in a position to accept orders for the new H. J. Mulliner all-metal lightweight saloon (duplicate of the forthcoming Show model) or the H. J. Mulliner special saloon, both with Bentley Mark VI chassis.—H. A. Fox & Co., Ltd., 2-5, Burlington Gardens, Old Bond St., W.1. Tel. No. Regent 7687.

FOX

RIPPON.

RIPPON.

RIPPON BROS., Ltd.

NORTHERN Bentley specialists.

1948 Mark VI standard saloon, black with air force blue leather.

1948 Mark VI standard saloon, pearl grey with blue leather.

1937 4½-litre 4-door sports saloon by Thrupp & Maberly, black with blue leather.

1935 3½-litre 4-door sports saloon by Park Ward.

1934 3½-litre 4-door sports saloon by Park Ward, black with green leather.

FOR further particulars contact the largest Bentley distributors.

RIPPON BROS., Ltd., Huddersfield 6340 (5 lines). Also at Bradford, Leeds and Sheffield. (0906)

DICKS CAR SALES offer:—

1937 Bentley 4½ drop head coupe, positively unmarked. Rolls record; £1,405.

DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maida Vale 6988-9. (7363)

CAR MART, Ltd.
BENTLEY 4½-litre 1948 razor-edge sports saloon by H. J. Mulliner, grey, 13,000 miles; £4,750.
CAR MART, Ltd., 250, Euston Rd., N.W.1. (1950)

BROWN'S for Bentley.
1937 Bentley 4½-litre Mayfair saloon de luxe, immaculate (excess) condition; £1,495.—Brown's Garage, Loughborough (Leicestershire) 4119 (Tubex). (1950)

TOM GARNER Ltd., offer
1947 (Dec) Bentley 4½-litre Mark VI standard steel saloon, grey with blue hide, recently serviced by makers, 26,000 miles. (1950)

1939 Bentley 4½-litre overdrive Haas Edge sports saloon by Freestone and Webb, dark grey with grey leather, radio, heater, discs and many extras, serviced regularly by makers, 56,000 miles. (1950)

TOM GARNER Ltd., 10-12, Peter St., Manchester, 2.
2 Blackfries 9265-6. (1950)

JACK OLDING, of Mayfair.
OFFICIAL Rolls-Royce and Bentley retailers,
OFFER:—

MARK VI Bentley Standard saloon, finished mistle-toe green with beige leather, 26,000 miles, first registered August 1948. (1950)

MARK VI Bentley Standard saloon, finished black with red leather, 16,800 miles, first registered May 1949. (1950)

MARK VI Bentley Standard saloon, finished black with brown leather, 32,000 miles, first registered January, 1948. (1950)

4½-litre Bentley saloon by Park Ward, finished black with green leather, first registered October, 1936, recent overhaul. (1950)

4½-litre Bentley four-door drop head coupe by Park Ward, first registered 1937, £10 tax. (1950)

4½-litre Bentley saloon by Mann Egerton, blue 4½ with blue leather, first registered October, 1936. (1950)

DETAILS of new and used Bentley cars for reasons—
AUDLEY House, 10, Audley St., W.1 Mayfair 5242. (1950)

NORTH Audley St., W.1 Mayfair 5242. (1950)

R. P. FUGGLE, Ltd., offer:—
1937 4½-litre Bentley 4-door sports saloon by A. Hooper, 67,000 miles, overhauled at Bentleys at £4,000; a most superb example of this model. (1950)

SWANMORE CAR SALES offer:—
1938 Bentley 4½ Park Ward B.L.E. series maroon, maroon hide, discs, radio and heater. (1950)

1937 Bentley 4½ Park Ward black and desert sand, recent £600 makers' overhaul. (1950)

1937 Bentley 4½ Freestone and Webb close coupled sports 4-door saloon, recent complete engine overhaul. (1950)

EXCHANGES, terms.—Swanmore Garage, 1176, E. Christchurch Rd., Boscombe. Tel. Southbourne 1022. (1950)

HAROLD RADFORD & Co., Ltd.,
INVITE you to call and inspect their unique selection of Bentley cars. (1950)

1949 (April) Bentley Mark VI chassis B.376.DA.
Raced with Countryman Towns and Country saloon body with folding rear seats, finished in natural polished mahogany and grey cellulose, with blue leather upholstery, mileage 9,000 only. (1950)

HAROLD RADFORD & Co., Ltd., Melton Court, South H. Kensington, S.W.7. Tel. Kensington 6642 (5 lines). (1950)

GUY SALMON AUTOMOBILES, Ltd., offer:—
1949 Bentley Mark VI, Aited 4-door sports saloon by James Young, 14,000 miles; £4,250. (1950)

1948 Bentley Mark VI standard saloon, 23,000 miles; £3,500.—Portsmouth Rd., Thames Ditton, Esherbrook SS15-2-5. (1950)

LARGE stock of 3½-4½ Bentley cars for disposal. (1950)

R. OWEN, Ltd., 17, Berkeley St., W.1. Mayfair 1060 (10 lines). (1950)

BENTLEY 4½ 1937-8 saloon.—22-24, Ot. Brook St., Birmingham 2, Ast. 3364. (1950)

CHARLES POLLETT, Ltd., accredited Bentley and Rolls-Royce retailers and repairers, offer:—
1949 Bentley Mk VI with special 4-door saloon by H. J. Mulliner metallic beige, pipskin upholstery, one owner, 18,000 miles; this is probably the most magnificent car ever built, and the extras include special wheel discs, horns, loose covers, etc.; original cost over £6,000; offered at £4,950. (1950)

18 SERVICE, Works and Stores: 12, Wellesley Ave., W.6. Riv. 1413. (1950)

1937 4½ razor-edged owner driver saloon, Thrupp & Maberly, small mileage, in superlative order throughout. (1950)

1936 4½ pillarless owner driver saloon by Vandenberg, one titled owner, an exceptional car, almost faultless.—R. C. Mortlake, 253, Kensal Rd., London, W.10. Leam 3125. (1950)

19000 miles.—1948 (April) Bentley Mark VI standard steel saloon, green.—Ernest Sutton, Cleve Hill 95 (Cheltenham). (Trade enquiries only please). (1950)

1934 Barker, recently checked over by Bentley agents; £285.—Working Motors (Maybury Hill), Ltd., 228, E. Christchurch Rd., Southbourne, 1022. (1950)

ROLLS-BENTLEY 3½-litre 4-door saloon, in excellent order, beautifully maintained, 2 owners, taxed year; 1930. Bank Dale, 66, Princess Gate Mews, Exhibition Rd., S.W.7, Kensington 6860. (1950)

1936 (Nov.) 4½-litre Bentley 4-door Thrupp & Maberly sunshine saloon, radio, exceptional condition, very attractive.—Lionel H. Pugh, 13-14, Brooks Mews, W.1. Mayfair 4433. (1950)

HOOPER & Co. (COACHBUILDERS), Ltd., 54, W.1. James's St., W.1. offer advice on the purchase of new and second-hand cars, and will be very pleased to show customers round their factory at Western Ave., Acton, W.3. Official Rolls-Royce and Bentley retailers. (1950)

BENTLEY car for sale, 4½-litre, 25.4hp sports saloon by Park Ward, finished black and cream with black hide upholstery, spacious luggage accommodation in rear, spare wheel discs, fitted radio, total mileage 52,350. 37, Palace Rd., Streatham Hill, S.W.2, Fulse Riv. 6404. (1950)

J. B. JACK BARCLAY, LIMITED.
LARGEST Official Retailers of Rolls-Royce and Bentley. Stock list of used models on request to 12-13, St. George St., Hanover Sq., London, W.1. (1950)

1935 4½-litre all-weather 4-seater coupe by James Young, one owner until 1950, Ace discs, tyres excellent, exceptionally pretty car; £1,250.—R. S. Mead, 42, Queen St. Maidhead, 2642. (1950)

1937 Bentley 4½-litre H.J. Mulliner 4-door Sports black saloon, leather upholstery, capacious boot, original owner, £1,000, exceptionally attractive, de-lightful condition. (1950)

1935 Series, chassis type B.C.R., Bentley 3½-litre saloon with Hodge coachwork; this has been the personal property of our managing director for the past four years, and is in really first-class mechanical condition; the 4-door body, finished in champagne and black, in modern in line and most attractive, taxed to December and on new oversize tyres here is a car to offer years of delightful carefree motoring; at £1,475 this Bentley is more expensive than some, but well worth the little extra; exchanges considered; full particulars from.—The Shires Motor Co., Ltd., Banbury, Tel. 2453. (1950)

BENTLEY (other than 3½ & 4½-litre)
BENTLEY 1929 4½ V.D.P., 3 owners, original condition throughout; £450; several reasons for selling.—Dove, 95, Woolwich Rd., S.E.2. (1950)

SPEED Six short chassis Bentley, open body, whole in very sound condition all through; £275.—Bentley Caravans, Priory Bridge Rd., Taunton. (1950)

BENTLEY 5-litre drop head coupe, 1936 chassis completely rebuilt, full four-seater resprayed, reupholstered, photos available; £550, consider small saloon exchange.—Box 6331. (1950)

ROLLS-BENTLEY 1937 4½-litre Park Ward saloon, engine completely overhauled recently at great cost, super condition, low mileage; £1,450, or would take first-class shooting brake or Land Rover in part exchange.—Box 6200. (1950)

1934 1932 4½ Bentley, fitted recently with modern A drop head, four-seater coachwork, almost the last Bentley made by the old company, grey with grey mohair head, everything spotless, mechanically beyond criticism, extensively chromium plated, Marchal lamps, plate clutch, vertical S.U.S. musical horns, the finest covered Bentley we have ever seen; very low price.—Speakers, Ltd., Crown Oak Lane, Salford, Mr. Redhill, Surrey. Horley 628. (1950)

BENTLEY 1929 4½-litre, short chassis open four-seater, in faultless mechanical condition, done less than 1,000 miles since rebores, coachwork immaculate with new tannous cover, hood and hood cover; £450; may be seen at Thurstone, Haxmans Hill, South-Borough, Kent. Southborough 615 or anywhere in southern counties from 26th to 29th September and after that at Park House, Shaw, Oldham. Shaw 7329 or anywhere by appointment. (1950)

Bentley Cars Wanted
OUR demand is urgent.
OWNERS who have Bentley cars for disposal are invited to communicate with the Swain Group of Companies, London offices, H. N. Owen, Ltd., 17, Berkeley St., W.1. Tel. Mayfair 9060. Head Office, 10, Audley St., W.1. Tel. Mayfair 5242. (1950)

THE CAR MART, Ltd., wish to purchase Bentley cars.—320, Euston Rd., N.W.1. Euston 1212. (1950)

BENTLEY
CAR specialist for over 20 years wishes to purchase pre- or post-war Bentleys in good condition; enquiries wanted; £1,000; £1,500; £2,000; £2,500; £3,000; £3,500; £4,000; £4,500; £5,000; £5,500; £6,000; £6,500; £7,000; £7,500; £8,000; £8,500; £9,000; £9,500; £10,000; £10,500; £11,000; £11,500; £12,000; £12,500; £13,000; £13,500; £14,000; £14,500; £15,000; £15,500; £16,000; £16,500; £17,000; £17,500; £18,000; £18,500; £19,000; £19,500; £20,000; £20,500; £21,000; £21,500; £22,000; £22,500; £23,000; £23,500; £24,000; £24,500; £25,000; £25,500; £26,000; £26,500; £27,000; £27,500; £28,000; £28,500; £29,000; £29,500; £30,000; £30,500; £31,000; £31,500; £32,000; £32,500; £33,000; £33,500; £34,000; £34,500; £35,000; £35,500; £36,000; £36,500; £37,000; £37,500; £38,000; £38,500; £39,000; £39,500; £40,000; £40,500; £41,000; £41,500; £42,000; £42,500; £43,000; £43,500; £44,000; £44,500; £45,000; £45,500; £46,000; £46,500; £47,000; £47,500; £48,000; £48,500; £49,000; £49,500; £50,000; £50,500; £51,000; 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Bugatti Spares and Service
J. LEMON BURTON, Bugatti service, 55, Netherwood St., N.W.6. Maids Vale 1531. 10071

SIMPSON'S MOTORS offer:-

1939 Buick as new, definitely immaculate motor car, mechanically perfect.
1939 Buick saloon, low mileage, immaculate motor car, £750.
1938 Buick American type Carlton drop head four-seater, fitted radio, heater, spotlight, immaculate condition throughout, £515.

SIMPSON'S MOTORS (WEMBLEY), Ltd. (American Car Specialists), Wembley 3303. 16235
THE CHESTER ENGINEERING Co., Ltd. offer:-

1948 Buick type 51 31hp Super Eight saloon, finished in dual grey with cloth upholstery, fitted radio and heater, mileage since new 4,500, this car is a replica of the show model and is in the most exceptional condition throughout; £2,150.
CITY Rd., Chester. Tel. Chester 23477. 17315

1936 Buick Regent saloon, complete engine overhaul recently, excellent tyres, best offer for quick sale.—Box 6308. 17091

£345—Buick 31 saloon, hide upholstery, radio, immaculate condition, £345. Value Cars, Ltd., 562, Upper Richmond Rd., East Sheen. 17798

LIMOUSINES—Roomy 8-seaters, not ex-hire, from £450.—Lawton-Goodman, 135, Cricklewood Broadway, N.W.2. Gladeside 2224. 17798

GORDON CARS (LONDON), Ltd.—1947 Buick 51hp saloon, grey, r.h. drive, 9,000 miles.—Gordon House, 373, Euston Road, E.C.1. 17441

1950—Buick 4-dr. saloon, in grey, 3,000 miles, radio, heater, etc., latest type with lamps in overdrive.—Syd Abrams, Ltd., Box 2254/7, 15548

1948 Buick 31 saloon, 1.5-litre power-operated drop head four-seater, as new, £2,350.—Park Garage (Molesey), Ltd., Hampton Court Way, Molesey, Tel. 4371. 17394

1947 (November) Buick 51, right-hand drive, 8,000 miles only; £2,850.—Clayton's Cars (London), Ltd., 357, Euston Rd., London, N.W.1. Tel. Euston 5229. 17394

1940 Buick 30.6hp Viceroy saloon, leather upholstery, sun roof, built-in radio, an immaculate car; £750.—Dollis Garages, Ltd., 42, Nether St., N.3 (3 mins. from Finsbury Central Station). Finchley 3130. 17015

1938 (November) Buick type B640, black with cloth upholstery, just completely reconditioned, fitted with radio, and whole car in exceptionally fine condition; £650.—Apply in first instance to Iron Bridge Service Dept., Tel. Underclay Rd., Southall, Tel. Southall 2355. We will demonstrate on owner's behalf.

BUICK 30hp, 1937, first registered 5/1/38, dark blue saloon, leather upholstery, mileage 15,000 only, the owner, this car practically as new, original cellulose which unmarked, and magnificent performance, taxed to December, the first to call will purchase this magnificent car; £725.—Dalwood Motors, Finner Green, Finner 670. 17931

LIMOUSINES 1939 Pullman, partition, 8-seater, black, immaculate order, £895. Below.

LIMOUSINES 1937 Double Enclosed, widest occasions, black, delightful condition, selection—£695. Seen—

LEE & SAUNDERS (100-Limousines: Lists posted).
A Providence Court, Grosvenor Square, 2941-Mayfair. 17589

Buick Cars Wanted
CASH immediately for good Buick.—H. P. Edwards, 154, St. Titchfield St., W.1. Langham 0013. 17762

SIMPSON'S MOTORS (WEMBLEY), Ltd. wish to purchase all models Buick, Wembley 3303. 17994

SOLE concessionaires, Lendrum & Hartman, Ltd., will purchase used Buick models.—Showrooms, Buick House, Albemarle St., London, W.1. Regent 7121. 10304

Buick Spares and Service
BUICK sole concessionaires, Lendrum & Hartman, Ltd., Buick Works, Old Oak Lane, Willesden Junction, N.W.10. Tel. Elgar 7911. 10141

CADILLAC
CADILLAC V.8 saloon, in excellent mechanical condition, coachwork and upholstery unmarked, engine steering and suspension recently overhauled, maintained regardless of time, new tyres all round, two spare wheels; £600.—Box 6408. 17229

Cadillac Cars Wanted
SIMPSON'S MOTORS (WEMBLEY), Ltd. wish to purchase all models Cadillac, Wembley 3303. 17994

SOLE concessionaires, Lendrum & Hartman, Ltd., will purchase used Cadillac models.—Showrooms, Buick House, Albemarle St., London, W.1. Regent 7121. 10304

Cadillac Spares and Service
CADILLAC sole concessionaires, Lendrum & Hartman, Ltd., Buick Works, Old Oak Lane, Willesden Junction, N.W.10. Tel. Elgar 7911. 10012

CHEVROLET
OVERSEAS CARS, Ltd.

1947 Chevrolet Fleetmaster saloon, maroon, left hand drive, £950.—For other Overseas Car Bargains see page 47. 17391

OVERSEAS CARS, Ltd. 227, Brompton Rd., Knightsbridge, S.W.3. Tel. Kensington 7475. 17668

DICKS CAR SALES offer:-

1948 Chevrolet 30hp saloon, 19,000 miles only, really as new; £1,350.
DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maids Vale 1531. 17371

CHEVROLET Fleetmaster, excellent condition, 28,000 miles, registered August, 1947; £1,075.—Stadium Autos, 178, New Barn St., E.15. Alb. 5566. 17261

1948 Chevrolet 4-door de luxe Fleetmaster saloon, 14,000 miles.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 3598. 17492

1939 Chevrolet Master 6 fixed head coupe, with occasional tests, coachwork unmarked; 17900
Petter Bantock Car Sales, 104, High Rd., Chiswick, W.4. Chiswick 2725/5870. 14403

£445—Magnificent Chevrolet de luxe saloon, special 24hp model, literally looks brand new, carefully used since 1935 by two careful owners, meticulously maintained, finest offered.

LAMBS OF WOOD GREEN, Finchley Showrooms, 421, High Road, N.12. Finchley 829. 17900

J. REYNOLDS (MOTORS), Ltd., Main Ford & Fordson Distributors, have pleasure offering from our huge stock of used cars of all types, a Chevrolet fixed head four-seater coupe 1939 30hp, this car has just been reconditioned and reconditioned where necessary, and is a most attractive car; £600.—Ford House, New Rd., Dagenham. Tel. Rainham 770 (8 lines). 16728

CHEVROLET Fleetmaster de luxe saloon, latest type, 1950 series, unquestionable condition throughout.—Autowork, Ltd., Winchester. Tel. Winchester 4354.

MAGNIFICENT power-operated four-seater Chevrolet convertible coupe in excellent condition throughout, speedometer reading only 39,000 miles, body finished in ivory and upholstered in red leather, fitted with many extras which include Motorola radio, Notex panchromatic, etc., etc.; first registered 1946 and taxed to end of year. Owner wishes to sell at £2,225 or would exchange for post-war 14-litre Riley, cash either way; to view apply—"Nelson Hotel," Victoria Rd. South, Southsea, Hants. Tel. evenings 5733. 17181

Chevrolet Cars Wanted
SIMPSON'S MOTORS (WEMBLEY), Ltd. wish to purchase all models Chevrolet, Wembley 3303. 17997

DISTRIBUTORS for London and Home Counties require good Chevrolet cars.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 3598. 17479

Chevrolet Spares and Service
CHEVROLET spares and repairs for private vehicles only; distributors for London and Home Counties. British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 3598. 17494

DICKS CAR SALES offer:-

1937 (September) Chrysler 25hp drop head four-seater coupe, fitted special body, positively indistinguishable from new, high grade car; £525.
DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, London, S.W.1. 17371

1938 25hp Chrysler 5-seater saloon; best offer.—Box No. 464, Dorlands, 18/20, Regent St., London, S.W.1. 17002

CHRYSLER saloon 1939 25hp, reconditioned engine just fitted, new battery, guaranteed to be in perfect condition in every way.—T. J. Strett, Bulmer's Court Nurseries, Epsom, Surrey. Tel. Epsom 1715

CHRYSLER 2-3-seater, 1932-33, but 1935 engine just installed, CD de luxe model carrying English "Coachman" body, classic vintage appearance, over £300 spent on it, receipts available, what offers?—Veterinary Surgeon, Leigh 200 (Lancs). 17148

Chrysler Cars Wanted
SIMPSON'S MOTORS (WEMBLEY), Ltd. wish to purchase all models Chrysler, Wembley 3303. 17996

ROWLAND SMITH'S, the Chrysler buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041.

FIRST-CLASS Chrysler cars urgently wanted.—Open & White (Leeds), Ltd., Roundhay Rd., Leeds, 7. Tel. 43405. 10567

Chrysler Spares and Service
CHRYSLER spare parts and engines for all models from stock.—Carmo, St. John's Wood Roundabout, N.W.1. Fri. 0141. 10018

CHRYSLER Specialists, repairs, spares, exchange engine service.—L. A. Mitchell (Motors), Ltd., Balham High Rd., S.W.12. Tel. Balham 2324. 17494

CITROEN
H.W. MOTORS, Ltd. offer:-

1949 Light 15 saloon de luxe, black, red upholstery, 14,000 miles, £1,600. 17878

H.W. MOTORS, Ltd. Walton-on-Thames 783 and 1437. 17878

R. F. FUGGLE, Ltd. offer:-

1949 Light 15 Citroen saloon with sun roof, colour black with beige leather, genuine 4,000 miles.
USHEY Heath, Herts. Tel. 1685. 17282

KENTISH & THOMSON offer:-

1947 (January) Citroen Light 15 saloon, black, cream wheels, loose covers, etc., a genuine French right-hand drive model in unusually fine order throughout. 17295

1946 (May) Citroen Light 15 saloon, black, brown leather, sliding head, one particularly fastidious owner since new. 17295

KENTISH & THOMSON, Ltd., 564-566, Wickham Rd., Shirley, Croydon, Springfield 3477-8. 17697

JOHN S. TRUBSCOTT, Ltd., for post-war Citroens; only first class cars offered. 17697

EXCHANGES deferred terms.—173, Westbourne Grove, W.11. Bay. 4274. 17419

WORTHINGTON MOTOR Co., Ltd., for Citroen sales, spares, service.—Tel. Worthing 71. 17419

1947 Citroen Light 15, privately owned, 30,000 miles, good condition, roof rack and seat covers, in excellent condition, best offer.—Box 6155. 16312

CITROEN 13hp 1938 works overhauled, reconditioned, perfect mechanical order and appearance, any test view West Country; best offer £400.—Box 6413. 17335

325—saloon, maroon, sliding head, red leather, very good condition; terms, exchanges; list, open 9-7 weekdays and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). Hampstead 6041. 17295

1947 Citroen 15 saloon de luxe, in perfect order, black with maroon leather interior, thoroughly overhauled (bills shown), radio and hosts of extras costing £85, one of the finest Citroens being offered for sale, small mileage; £825 or nearest; terms.—13, Church St., Epsom 6212. 16451

Citroen Cars Wanted
C. O. NORMAN & Co.

CITROEN Sole distributors for the County of London.

BUYERS of low-mileage Citroen cars.—46-52, Vauxhall Bridge Rd., London, S.W.1. Victoria 7611-8.

KENTISH & THOMSON offer:-

ARE probably the best buyers of really good post-war cars. 17295

KENTISH & THOMSON, Ltd., 564-566, Wickham Rd., Shirley, Croydon, Springfield 3477-8. 17697

WANTED privately, post-war Citroen.—14, Wood Lane, Lydney, Todmorden, (M6) 1837. 16863

REQUIRE post-war Citroen urgently.—30, Ryecliff Rd., S.W.16. Tulse Hill 1248. 15805

WANTED post-war Citroen Light 15, no dealers, J. Isen, 94, Dilton Fines, Cambridge. 16794

H.W. MOTORS, Ltd. always require first-class Citroen.—Tel. Walton-on-Thames 783 and 1437.

ROWLAND SMITH'S the Citroen buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041.

POST-WAR Citroen required, cash payment.—Morris 54, St. James Hill, S.W.2. Tulse Hill 4458.

JOHN S. TRUBSCOTT, Ltd. urgently require first-class Citroens.—173, Westbourne Grove, W.11. Bay. 4274. 17423

ACE SERVICE STATION (LONDON), Ltd. purchase for cash all Citroen cars.—North Circular Rd., Stonebridge Park, N.W.10. Elgar 5585 (5 lines). 16774

Citroen Spares and Service
WIDCOMB GARAGES, Ltd., Putney Rd., Bath 48-hr. service. 17923

C. O. NORMAN & Co.

CITROEN sole distributors for the County of London, Citroen spares and service.—50, Vauxhall Bridge Rd., S.W.1. Victoria 7611-8. 10706

SALES of Croydon.—Distributors and specialists for over 25 years. Tel. Croydon 5131-3. 16189

B will recondition as new.

THE HEADINGLEY MOTOR & ENG. CO., Ltd. & Olley Rd., Leeds. Tel. 32627-8. Grams. Trub. 17006

CITROEN specialists, £10 tax, a perfect exchange gear box fitted 24 hours.—Lorraine Garage, 29-31, Sionton Mews, S. Kensington, S.W.7. Western 6970.

CITROEN—We are distributors for N.W. Kent and 7 specialists in reconditioning these cars; front drive assemblies fitted from stock.—Barnhurst Garage, Ltd., Bexleyheath 725.

ROD ROAD GARAGE & ENGINEERING Co., Ltd., B. Bowes Rd., N.11 (Bow, 2284), specialists on Citroen body repairs and mechanical overhauls; swivel joints reconditioned 48-hr; all spares stocked. 17679

CARR BROS. offer:-

1946 Daimler 2 1/2-litre, 16,000 miles only, black cellulose, as new, interior luxuriously upholstered best quality leather, £10 tax, a perfect specimen of a luxury car offered at a very reasonable price; £1,025. 17680

CARR BROS. GARAGES, Purley. Uplands 4811/2. 17680

HAROLD RADFORD & Co., Ltd.

1948 (July) Daimler 2 1/2-litre two-door drop head coupe, finished in black with red leather upholstery, heater and demister, one owner, 21,000 miles, in exceptional condition throughout. 17681

HAROLD RADFORD & Co., Ltd., Melton Court, South Kensington, S.W.7. Tel. Kensington 6642 (5 lines). 17681

PALMER'S MOTORS, Ltd. offer:-

1938 Daimler 25hp 6-light saloon, Ace discs, £10 tax; £750; phone, call or write—£35, York St., Twickenham. 17087. 17979

WARWICK WRIGHT, Ltd. offer:-

1948 Daimler 2 1/2-litre saloon, black, brown leather, 12,000 miles; £1,550.

WARWICK WRIGHT, Ltd., 150, New Bond St., W.1. Mayfair 9761. 17683

STRATSTONE, Ltd., Daimler specialists offer:-

1946 Daimler 2 1/2-litre saloon, radio, loose covers; very well maintained; £1,450. 17684

1939 Daimler 20hp saloon, black with brown leather, one owner, 33,000 miles only; excellent condition; £625.

1937 small mileage, most attractive throughout; £550.

STRATSTONE, 40, Berkeley St., W.1 (Mayfair 4494), Tel. 7 Herbrand St., Russell Sq., W.C.1. 17290

RUSSELL MOTORS (KNIGHTSBRIDGE), Ltd.

1937 Daimler E.L. series 24hp 4-door drop head coupe by Tickford, 56,000 miles only; any trial or examination.—47, Sloane St., S.W.1. Tel. Sloane 2912. 17684

1949 Daimler 2 1/2-litre saloon, 5,000 miles.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. 17494

1938 Daimler 4 1/2-litre Straight 8 Hooper limousine, first registered 1948, £10 tax.—J. B. Tansbourne, 50, Wilton Row, S.W.1. Sloane 4663. 16135

D sprayed; £665.—Morton Engineering Co., Ltd., Faggs Rd., Feltham, Middlesex. Tel. Feltham 3045.

SPINK (BOURNEMOUTH), Ltd., engaged exclusively to the distribution of Daimler cars, please consult us when buying or selling; all spares and every service. 17684

DAIMLER House, Bournemouth. Tel. 5405. 10544

ROSE & YOUNG, Ltd. offer 1936 model Daimler 20 saloon, black, blue leather; £325.—63-65, Strand, W.C.2. Q. minute Sireham Hill Station. Tulse Hill 6464. 17656

1938 (model) Daimler saloon 16.2hp, 24 mps, two-toned, 1938, 1939, in amazing condition; £695; any trial.—Kingston Garage, Springfield, Ansbury near Hull. Tel. 48882. 17436

DAIMLER landaulets, 5, 35hp Daimler 7-seater engines, 1938, 1939, in amazing condition; £250 each; for the three; these cars have been in regular daily use.—Coventry & Jeffs, Ltd., St. George Rd., Bristol. 17494

1949 2 1/2-litre Daimler saloon, 19316, black with green leather upholstery, 15,000 miles only; £1,550 or near offer.—W. H. Arthur & Co., Ltd., Station Garage, Billericay, Essex. Tel. Billericay 110.

1949 Hooper razor edged saloon, electric division, black/blue leather, radio and every luxury, magnificent motor car.—Sidney Marcus, Ltd., 33, Sloane St., S.W.1. Sloane 3557-9870. 17684

35—120 Daimler 7-seater car, 2 spares, paintwork unmarked, modern wings and running boards, tax for 1935, £175, also offer.—The Regent Inn and Service Garage, 291-293, Ballards Lane, North Finchley, N.12. Hillside 4405-4011. 17683

£595—Daimler 2-litre saloon 1939, finished in original black with blue cellulose with leather interior, privately owned since new, the log book showing one owner only, fully guaranteed in writing and a most outstanding opportunity at the price.

DAIMLER MOTORS, Ltd., Leighton Buzzard, Beds. C. Tel. 2041-2-3. Write for our 18-page, post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Mon-Sat. inclusive. 17851

MAGNIFICENT rebuilt Daimler for disposal, property of midland managing director, one of the most "2.30" series (20hp), completely stripped out of chassis and rebuilt at Daimler works, just delivered as new, £800 spent on car; first offer of £1,500 accepted; viewing by appointment in Birmingham. Box 6326. 17119

LIMOUSINES 1939/40 Double Enclosed, widest of occasions, best of exceptional order, reasonable cost.

A LPE & SAUNDERS (100-Limousines: Lists posted).
A Providence Court, Grosvenor Square, 2941-Mayfair.

Daimler Cars Wanted

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THE CAR-MART, Ltd. wish to purchase Daimler cars.—150, Park Lane, W.1. Grosvenor 3434. 10959

ROWLAND SMITH'S, the Daimler buyers.—Hampton Road, High St. (Hampton Tube). Tel. 6041. 19532
CASH immediately for good Daimler.—H. F. Edwards, 154, Gt. Titchfield St., W.1. Langham 4012. 17765
DAIMLER and **COROLLA MOTORS**, Ltd., require good Daimler cars.—Upper St. Martin's Lane, W.C.2. Tel. 2598. 17473

Daimler Spares and Service

ACROTT MOTORS, Ltd.—Pre-selector gear boxes; exchange and repairs.—169, Fulham Rd., S.W.3. Kensington 7309. 18034
ACROTT MOTORS, Ltd.—Donald Vince & Co., Ltd., Daimler and Lanchester specialists, for sales and service.—Kidderminster Rd., Cropton 5775. 10686
SPECIALISED Daimler rear box service; all types quick deliveries, guaranteed work, spare parts supplied.—H. & A. Engineering, 35, Grant Rd., Cropton, Surrey. Addlecombe 2251. 16145
DAIMLER and Lanchester spares.—Large stock of spares for most models; specialists in spares unit, pistons, etc., for the Daimler sleeve valve series.—Allans, Victoria Rise, Clapham. S.W.8. Macclesfield 4199 and 4874. 16145

DELAGE

J. MARSHALL offers:—
1933 D85 Delage 2-4-seater, a delight to motor and a joy to observe; £550.
J. MARSHALL, 869, St. Albans Rd., Watford. Tel. 3729. 17635
1950—Delage D6 Chapron saloon, May 1931, 64,000 miles.—Richards & Brown, 2, Ringers Rd., Bromley. Tel. Haveringwood 7487 & 6479. 17635
DELAGE spares and service
SELBORNE (MAYFAIR), Ltd. (Incorporating Selborne Motors, Ltd., established 1926).
RAPID repair and parts for Delage.
SOLE concessionaires for Great Britain, etc.
HEAD office, 82, Park St., W.1. 16495
WE buy, sell and repair Delages.—Marlborough Garage, Abbey Rd., St. John's Wood N.W.8. Tel. 3729. 17635

DELAHAYE

1949 Delahaye four-door drop head coupe 3½-litre, entirely new condition.—H. Buss, Woodall & Sons, Ltd., Woodall St. 2158. 16795
ROSE & YOUNG, Ltd., offer Delahaye 3½-litre sports saloon, 4-door, black, leather upholstery, first registered 29/12/35, very attractive car; £775.—45-49, St. Michael's Ave., Streatham Hill, S.W.2. (1 minute Streatham Hill Station). Tel. Hill 6444. 17632
DELHAYE spares and service
SELBORNE (MAYFAIR), Ltd. (Incorporating Selborne Motors, Ltd., established 1926).
SOLE concessionaires for the famous Delahaye cars.
U.S.A. and other overseas enquiries invited.
HEAD Office: Regional House, 82, Park St., W.1. 16495

D.K.W.

225 c.c.—D.K.W. (Aug. 1936) 7hp saloon, black and red, used, carefully used, very good condition, taxed; terms exchange.—Rowland Smith, below.
165 c.c.—D.K.W. 1934 5hp convertible saloon, black and cream, carefully used, excellent condition; terms exchange, our specialist.—S. M. Garages, Ltd., 40a, St. Michael's St., W.2. Paddington 6777. 10016
D.K.W. spares and service
NEW big-end and mains fitted to D.K.W. crankshafts.
C. F. SMITH & Co., 83-85, St. John's Hill, Clapham Junction. Tel. 6071. 10066

Dodge Spares and Service

DODGE specialists, repairs, spares, exchange engines.—Church Road Engineering Co., Ltd., Dodge Distributors, Huddell, Essex. Tel. 59474/57127. 10156
D service.—L. A. Mitchell (Motors), Ltd., Balham High Rd., London, S.W.12. Tel. Balham 2434. 10362

FIAT

1939 Fiat 500 four-seater saloon, excellent condition; £260.
ABBEY AUTOS, rear of 44-46, Chase Side, Southgate, N.14 (near Tube). Palmer Green 4540. 17689
FIAT 500 1937-4 model, excellent condition, low mileage; what offers?—Box 6156. 16324
1938 500 Fiat drop head coupe, Derrington reconditioned engine being fitted now; £275.—Box 6415. 17510
CATEHOUSE offer 1937 Fiat 500 saloon, showroom condition; £325.—Gateshouse Motors, Ltd., Highgate Village, London, N.6. Mountview 4444. 17716
1500 Fiat 2 1/4-seater Ghia drop head, re-chromed, engine as new; £450.—Phillips, Chase, Sibbs Hedingham, Essex. 16799

1940 Fiat 1500 drop head coupe, just repainted, maroon, superb and rare motor car; £585.—Bells Service Garages, 144, London Rd., Kingston-on-Thames 1185. 17997

1938 Fiat 1100 pillarless saloon, finished blue, good appearance with first-class performance; £425.—Bells Service Garages, 144, London Rd., Kingston-on-Thames 1185. 17997

MAYFAIR GARAGES, Ltd.—1939 1100 4-door pillarless sunning saloon, grey hide interior, good tyres, very smart car, in excellent condition throughout, 3 months guarantee; £425.—Below.

M pillarless sliding head saloon, maroon, leather upholstery, bumpers, very good condition, 3 months guarantee; £245.—Below.

MAYFAIR GARAGES, Ltd.—August 1939, 500 de luxe 4-seater saloon, mistletoe green, hide upholstery, all good tyres, well maintained, excellent condition throughout; £390.—Below.

MAYFAIR GARAGES, Ltd.—June 1939, 500 de luxe convertible coupe, jewelencement silver, blue leather, all good tyres, excellent condition throughout, 3 months guarantee; £390.—Below.

MAYFAIR GARAGES, Ltd.—1938 model 500 de luxe convertible coupe, silver with red leather upholstery, good tyres, well maintained, 3 months guarantee; £390.—Below.

MAYFAIR GARAGES, Ltd.—Fourteen Fiat in stock; price list and road test report upon request.
MAYFAIR Balclutha, Ltd., Balclutha St. (opp. Selridge's clock), Mayfair, W.1. Mayfair 5104-5. Open 9-6. Sats. 9-1. 17458

1938 Fiat 500 coupe, black with blue upholstery, good tyres, in very nice condition throughout; £280; 3 months guarantee.—Tankard & Smith, Ltd., 37, Peckham Rd., S.E.15. Rodney 2051. 16942

£198 cash or £65 deposit.—1937 Fiat 500 coupe, clean, right blue cellulose with red leather upholstery, good condition throughout; terms quickly arranged, open 3-6 week-days, 9-6. S. & S. Motors, Ltd., Dudden Hill Lane, Hendon. Gladstone 8665-6. 17913

Fiat Cars Wanted

ROWLAND SMITH'S, the Fiat buyers.—Hampton Road, High St. (Hampton Tube). Ham. 6041. 10333

CASH immediately for good Fiat.—H. F. Edwards, 154, Gt. Titchfield St., W.1. Langham 4012. 17766
MAYFAIR GARAGES, Ltd., Haverdon St. (opposite Selfridges), W.1. Mayfair 3104. Particularly want 500s, 1100s and 1500s for cash. Phone or write for buyer to call. 17446

Fiat Spares and Service

FIAT 500cc genuine Fiat spares, reconditioned engines, gear boxes, starters, dynamos, etc., guaranteed repairs.—S. & S. Motors, 165a, Westbourne Grove, W.11. Tel. Bay 1642. 10156

FIAT (ENGLAND), Ltd., Water Rd., Wembley, sole licensees for the United Kingdom, are only sources of genuine Fiat spares and service.—Tel. Ferivale 5651. Grams: Fiat, Wembley. 10009

B.D.J. (ENGLAND), Ltd., offer exchange engines, 4-cyl. 500 cc, 1100 cc, 1500 cc, 1900 cc, 2000 cc, 2300 cc, 2600 cc, 2800 cc, 3000 cc, 3200 cc, 3500 cc, 3800 cc, 4000 cc, 4200 cc, 4500 cc, 4800 cc, 5000 cc, 5200 cc, 5500 cc, 5800 cc, 6000 cc, 6200 cc, 6500 cc, 6800 cc, 7000 cc, 7200 cc, 7500 cc, 7800 cc, 8000 cc, 8200 cc, 8500 cc, 8800 cc, 9000 cc, 9200 cc, 9500 cc, 9800 cc, 10000 cc, 10200 cc, 10500 cc, 10800 cc, 11000 cc, 11200 cc, 11500 cc, 11800 cc, 12000 cc, 12200 cc, 12500 cc, 12800 cc, 13000 cc, 13200 cc, 13500 cc, 13800 cc, 14000 cc, 14200 cc, 14500 cc, 14800 cc, 15000 cc, 15200 cc, 15500 cc, 15800 cc, 16000 cc, 16200 cc, 16500 cc, 16800 cc, 17000 cc, 17200 cc, 17500 cc, 17800 cc, 18000 cc, 18200 cc, 18500 cc, 18800 cc, 19000 cc, 19200 cc, 19500 cc, 19800 cc, 20000 cc, 20200 cc, 20500 cc, 20800 cc, 21000 cc, 21200 cc, 21500 cc, 21800 cc, 22000 cc, 22200 cc, 22500 cc, 22800 cc, 23000 cc, 23200 cc, 23500 cc, 23800 cc, 24000 cc, 24200 cc, 24500 cc, 24800 cc, 25000 cc, 25200 cc, 25500 cc, 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1949 (June) Ford Prefect saloon, green, fawn interior, 4,500 miles, one owner, as new.—Tickford, Ltd., 4, Upper St. Martin's Lane. Tel. 7387
Temple Bar 5338

1947 Ford Prefect saloon, black/grey cloth, 21,000 miles, new engine fitted, £575.—L. F. Dove, Limited, Mid-Surrey Car Centre, Guildford Road, Woking, Tel. 1282.

1949 (Aug.) Ford Prefect saloon, black, brown cloth, guaranteed 6,000 mls., as new, £750; 1945 (Feb.) Ford Prefect saloon, black, brown cloth, 15,000 mls., £650.—Glosson Sports Cars (Christchurch), Ltd., Lynchdurst Rd., Christchurch, Hants. Tel. 1681.

Ford Ten Cars Wanted

I REQUIRE post-war Ford 10 urgently.—30, Ryecroft Rd., S.W.16, Tulse Hill 1288. Tel. 2776

ROWLAND SMITH'S, the Ford 10 buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041. (0935)

1949 Ford 10 Prefect saloon required.—Turnbull, Cross House, Station Hill, Winchester, 7501

CASH buyers of low mileage Ford 10s; distance no object.—Hattoms, Lord St., Southampton. Tel. 2268

MARSTON MOTORS, Ltd., for your Ford 10. Tel. Sta. 8000. 1000 Slaters Rd., Tottenham, N.15.

POST-WAR Ford 10 required, cash payment.—Morley, 54, Streatham Hill, S.W.2, Tulse Hill 4898.

Wish to purchase small size Ford 10, new type, saloons.—Brown & Mallalieu, Ltd., Blackpool, Tel. 22322.

DAGENHAM MOTORS, Ltd., main dealers, purchase Ford 10hp cars for cash.—56, Park Lane, London, W.1. Regent 4866. (0156)

1939—1949 Ford 10 saloon urgently required.—3, Hard Place, Epsom, Surrey, S.24. 4916

Tottenham, N.15. Tel. 2332 and 0464. (4916)

RAYMOND WAY, the hire-purchase specialists, are still buying Ford 10, and have unlimited cash available.—Cattermole Rd., Kilburn, N.W.8. Haida Vale 6044 (10 lines). (5660)

FORD (V.8)

HAROLD PERRY, Ltd., Invicta Works, 279, Ballards Lane, North Finchley, N.12. Tel. Hillside 4444.

1948 Ford, colour black, leather interior, complete with radio and heater; £950

1948 Ford Pilot saloon, colour black, leather interior, complete with radio; £950.

Warlds Lane, North Finchley, N.12. Tel. Hillside 4444. (7533)

LAYTONS OF OXFORD offer:—

1945—1948 Ford Pilot saloon, very low mileage and in new condition throughout; black with grey Bedford coach upholstery; terms if required.

LAYTONS OF OXFORD (MOTORS), Ltd., New Rd., Oxford, Tel. 3581. (7242)

WANSTEAD MOTORS, Ltd., offer:—

1948 Ford Pilot, black, brown leather, 15,000 miles; £925.

WANSTEAD MOTORS, Ltd., Cambridge Park, E.11. Wanted 1000. (7250)

MANN EGERTON & Co., Ltd., offer:—

1948 Ford V.8 Pilot, blue with blue leather upholstery, radio and heater fitted, 13,000 miles; £995.

14, Berkeley St., London, W.1. Regent 2073. (7332)

DAGENHAM MOTORS, Ltd., Ford Main Dealers.

1948 Ford Pilot, blue, blue hide, radio, 8,000 miles.

56, Park Lane, W.1. Regent 4866; 374, Ealing Rd., Alperton, Middlesex, Tel. 5393; and 9, 12, 14, 16, 18, 20, 22, 24, 26, 28, 30, 32, 34, 36, 38, 40, 42, 44, 46, 48, 50, 52, 54, 56, 58, 60, 62, 64, 66, 68, 70, 72, 74, 76, 78, 80, 82, 84, 86, 88, 90, 92, 94, 96, 98, 100, 102, 104, 106, 108, 110, 112, 114, 116, 118, 120, 122, 124, 126, 128, 130, 132, 134, 136, 138, 140, 142, 144, 146, 148, 150, 152, 154, 156, 158, 160, 162, 164, 166, 168, 170, 172, 174, 176, 178, 180, 182, 184, 186, 188, 190, 192, 194, 196, 198, 200, 202, 204, 206, 208, 210, 212, 214, 216, 218, 220, 222, 224, 226, 228, 230, 232, 234, 236, 238, 240, 242, 244, 246, 248, 250, 252, 254, 256, 258, 260, 262, 264, 266, 268, 270, 272, 274, 276, 278, 280, 282, 284, 286, 288, 290, 292, 294, 296, 298, 300, 302, 304, 306, 308, 310, 312, 314, 316, 318, 320, 322, 324, 326, 328, 330, 332, 334, 336, 338, 340, 342, 344, 346, 348, 350, 352, 354, 356, 358, 360, 362, 364, 366, 368, 370, 372, 374, 376, 378, 380, 382, 384, 386, 388, 390, 392, 394, 396, 398, 400, 402, 404, 406, 408, 410, 412, 414, 416, 418, 420, 422, 424, 426, 428, 430, 432, 434, 436, 438, 440, 442, 444, 446, 448, 450, 452, 454, 456, 458, 460, 462, 464, 466, 468, 470, 472, 474, 476, 478, 480, 482, 484, 486, 488, 490, 492, 494, 496, 498, 500, 502, 504, 506, 508, 510, 512, 514, 516, 518, 520, 522, 524, 526, 528, 530, 532, 534, 536, 538, 540, 542, 544, 546, 548, 550, 552, 554, 556, 558, 560, 562, 564, 566, 568, 570, 572, 574, 576, 578, 580, 582, 584, 586, 588, 590, 592, 594, 596, 598, 600, 602, 604, 606, 608, 610, 612, 614, 616, 618, 620, 622, 624, 626, 628, 630, 632, 634, 636, 638, 640, 642, 644, 646, 648, 650, 652, 654, 656, 658, 660, 662, 664, 666, 668, 670, 672, 674, 676, 678, 680, 682, 684, 686, 688, 690, 692, 694, 696, 698, 700, 702, 704, 706, 708, 710, 712, 714, 716, 718, 720, 722, 724, 726, 728, 730, 732, 734, 736, 738, 740, 742, 744, 746, 748, 750, 752, 754, 756, 758, 760, 762, 764, 766, 768, 770, 772, 774, 776, 778, 780, 782, 784, 786, 788, 790, 792, 794, 796, 798, 800, 802, 804, 806, 808, 810, 812, 814, 816, 818, 820, 822, 824, 826, 828, 830, 832, 834, 836, 838, 840, 842, 844, 846, 848, 850, 852, 854, 856, 858, 860, 862, 864, 866, 868, 870, 872, 874, 876, 878, 880, 882, 884, 886, 888, 890, 892, 894, 896, 898, 900, 902, 904, 906, 908, 910, 912, 914, 916, 918, 920, 922, 924, 926, 928, 930, 932, 934, 936, 938, 940, 942, 944, 946, 948, 950, 952, 954, 956, 958, 960, 962, 964, 966, 968, 970, 972, 974, 976, 978, 980, 982, 984, 986, 988, 990, 992, 994, 996, 998, 1000.

1947 Ford Pilot saloon, blue, 6,000 miles.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tem. 3598.

1949 Ford V.8 Pilot, 1946, blue, leather, radio, heater, as new, £935.—B. Tansbourne, 30, Winton Row, S.W.1. Sloane 4665. (6136)

1949 Ford Pilot, 1949, green, fawn leather, radio, heater, low mileage, excellent condition, £850.—On view at Linguists Club, 20, Grosvenor Place, S.W.1. Slo. 9595.

1938—1938 81A Ford 50hp drop head fourseater coupe, grey, new hood, very attractive.—Ray Motors, 180-184, West End Lane, N.W.8. Hampstead 6990.

1949—1935 Ford V.8 saloon one owner, new steering rack, steering 1947 new brakes, shock absorbers June 1950, licensed Dec. 31; any trial.—Dr. Gree, 585, Erith Rd., Erith. (6979)

1949—1938 Ford V.8 coupe, new engine recently fitted, special anti-roll bars to support and new Rotoflo shock absorbers, new tyres all round, fitted radio and heater, a most genuine car.

1949—1938 81A Ford 50hp drop head fourseater coupe, grey, new hood, very attractive.—Ray Motors, 180-184, West End Lane, N.W.8. Hampstead 6990.

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1949—1938 81A Ford 50hp drop head fourseater coupe, grey, new hood, very attractive.—Ray Motors, 180-184, West End Lane, N.W.8. Hampstead 6990.

FORD V.8 Mercury 32hp saloon, first registered 1948, black and chrome with brown leather and carpets, dove grey and red interior fittings, a very fast, reliable and economical car which is fitted with many extras including latest type P.V. built-in radio, twin spot lamps, interior light, rear locker, reversing light etc., etc., seating comfort for 6 people; undoubtedly one of the smartest and best conditioned of these cars in the country; £925.—McNellie, Pooks Hill Cottage, Grayshott, Hindhead, Surrey. (7076)

Ford V.8 Cars Wanted

SIMPSON'S MOTORS (WEMBLEY), Ltd., urgently require all model Ford V.8s.—Wembley 3903. (9148)

ROWLAND SMITH'S, the Ford V.8 buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041. (0935)

JOHN S. TRUSCOTT, Ltd., urgently require first-class Ford Pilots.—173, Westbourne Grove, W.11. Bay. 4274. (7425)

DAGENHAM MOTORS, Ltd., main dealers, purchase Ford V.8 cars for cash.—56, Park Lane, London, W.1. Regent 4866. (0156)

UTILITY—FORD OR OTHER BODIES

1947 (November) Ford 8 utility, exceptionally smart; 2455.—Jacquet, Ltd., 225-7, Ham-mermith Rd., W.6. Riverside 6677-8. (7649)

1939—1939 Ford V.8 50hp Martin Walter 8-seater Utility, reconditioned engine, re-equipped, excellent condition.—Stray Motors, 180-184, West End Lane, N.W.8. Hampstead 6990. (8044)

1949 Fordson 5/6wt and 10wt Utilities, 1949, 1948, 1947, 1946, 1945, 1944, 1943, 1942, 1941, 1940, 1939, 1938, 1937, 1936, 1935, 1934, 1933, 1932, 1931, 1930, 1929, 1928, 1927, 1926, 1925, 1924, 1923, 1922, 1921, 1920, 1919, 1918, 1917, 1916, 1915, 1914, 1913, 1912, 1911, 1910, 1909, 1908, 1907, 1906, 1905, 1904, 1903, 1902, 1901, 1900, 1899, 1898, 1897, 1896, 1895, 1894, 1893, 1892, 1891, 1890, 1889, 1888, 1887, 1886, 1885, 1884, 1883, 1882, 1881, 1880, 1879, 1878, 1877, 1876, 1875, 1874, 1873, 1872, 1871, 1870, 1869, 1868, 1867, 1866, 1865, 1864, 1863, 1862, 1861, 1860, 1859, 1858, 1857, 1856, 1855, 1854, 1853, 1852, 1851, 1850, 1849, 1848, 1847, 1846, 1845, 1844, 1843, 1842, 1841, 1840, 1839, 1838, 1837, 1836, 1835, 1834, 1833, 1832, 1831, 1830, 1829, 1828, 1827, 1826, 1825, 1824, 1823, 1822, 1821, 1820, 1819, 1818, 1817, 1816, 1815, 1814, 1813, 1812, 1811, 1810, 1809, 1808, 1807, 1806, 1805, 1804, 1803, 1802, 1801, 1800, 1799, 1798, 1797, 1796, 1795, 1794, 1793, 1792, 1791, 1790, 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1125, 1124, 1123, 1122, 1121, 1120, 1119, 1118, 1117, 1116, 1115, 1114, 1113, 1112, 1111, 1110, 1109, 1108, 1107, 1106, 1105, 1104, 1103, 1102, 1101, 1100, 1099, 1098, 1097, 1096, 1095, 1094, 1093, 1092, 1091, 1090, 1089, 1088, 1087, 1086, 1085, 1084, 1083, 1082, 1081, 1080, 1079, 1078, 1077, 1076, 1075, 1074, 1073, 1072, 1071, 1070, 1069, 1068, 1067,

1949 Humber Super Snipe, black/brown leather, 8,000 miles, one owner, superb condition throughout; £1,495.—Odeon Motors, Ltd., Barrack 4100.

1947 Humber Pullman limousine, in excellent condition, can be seen by appointment.—Merchants Garage, Dorchester, Dorset. Tel. Dorchester 540-1. 17090

1947 Humber Snipe 18hp, finished maroon, 18,000 miles, showroom condition throughout; £950.—Bells Service Garages, 144, London Rd., Kingston-on-Thames T165.

27hp 1939 Humber Snipe, in first-class condition, small mileage with new engine, four new tyres, taxed up to end of year; £500 or near offer.—W. Haynes, Bletchington, Oxon. 17073

1939 Humber 27hp Imperial saloon, winding division with occasional tables, low mileage, excellent condition, open to any trial.—The County Garage, Gerrards Cross 2279/3725.

11000 miles only since new, Humber 18, 1937, black, beige hide throughout, 1947 appearance and condition, one owner; £595; small saloon exchange, cash adjustment.—Western 1225 7817

1936 Humber Snipe 7-seater saloon, fitted with latest type Perkins P.6 diesel engine (8,000 miles), new gear box (2,000 miles) and service diff. unit (1,000 miles), brakes renewed, tyres in good condition all through; price £625.—Rico Engineering, Ltd., 32, Linaker Street, Southport. 17063

£650—Humber Pullman 27hp Thrupp and Maberly enclosed limousine 1939-9, finished in black and chrome, with very elegant interior furnishings of the highest quality, fully equipped with face forward occasional seats, winding division, bright and dim reading lamps, inter com., heater front and rear, very spacious external rear boot and additional luggage space provided, twin headlights and every luxury, equally suitable for high-class country work, weddings, funerals, etc., an opportunity that should not be missed. **CAMDEN MOTORS**, Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Write for our 18-page, post free catalogue of over 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive. 17867

LIMOUSINE (late 1948) Pullman, widest occasional, black, mileage 18,000, beautiful order, LPE & SANDERS (100 Limousines, Lists posted), Providence Court, Grosvenor Square. 2941-Mayfair. **Humber Cars Wanted**

R. ROOTES Ltd.
DISTRIBUTORS.
ACQUIRE modern low-mileage Humber cars.

IRMINGHAM—Lower Temple St. (Central 8411.)
MANCHESTER—129, Deansgate. (Blackfriars 6677.)
MAIDSTONE—(Maidstone 3333.)

CANTERBURY—(Canterbury 3232.)
COCHESTER—(Chatham 2231.)
WROTHAM Heath—(Borough Green 4.)

ROOTES Ltd., Devonshire House Piccadilly, W.1. Tel. Grosvenor 3401.

ROWLAND SMITH'S, the Humber buyers.—Hamstead High St. (Hamstead Tube). Ham. 6041.
CYRIL SHEPARD requires:—

HUMBERS, all models.—102, King's Rd., Reading 2712.
POST-WAR Humber Pullman urgently required.—Full details and price to Box 6162. 16859

CASH buyers, low mileage Humber Hawks; distance no object. Hattons, Lord St., Southampton. Tel. 2268. 10793

BRITISH & COLONIAL MOTORS, Ltd., require good Humber cars. Upper St. Martin's Lane, W.C.2. Tem. 3598. 17472

URGENTLY required, late type Humber Hawk, low mileage, no dealers.—J. Ison, 94, Dilton Fields, Cambridge. 16786

BIRMINGHAM and Midlands.—Low-mileage Humber modern cars required by George Heath, Ltd., 180-184, Newhall St., Birmingham and Lower Temple St., Birmingham. 2. 10066

CRIPPS, of Nottingham, urgently require all types of Humber cars, recent models, send full particulars to: Cripps & Co., Ltd., The Humber Distributors, Parliament St., Nottingham. Tel. 44558. 10553

THE Humber specialists for all spares—Ring Uplands 3637. See advert under parts and accessories. 18268

ENGINES and gear boxes, reconditioned exchange units 1934-48; trade or retail.—Galway Services, Ltd., Dromic St., Leeds 11. Tel. 25690. 10345

DE NORMANVILLE gear box overhauls; spare parts supplied; recommended specialists.—H. & A. Engineering 35, Grant Rd., Epsom, Surrey. 17074

INVTICA 1½-litre 12hp saloon, 1932, rebored.—Hunter, 118, University Ave., Glasgow. 17178

F.N. Ltd., offer a number of new Black Prince 1½-litre chassis, partially completed, for the enthusiast wishing to complete.—Falcon Works, London Rd., Islington, Hounslow 0011. 15343

LONDON CARS offer:—

1939 1½-litre S.S. Jaguar, black, green interior, first-class condition throughout, taxed 31/12/39, guaranteed; £478.

1938 2½-litre S.S. Jaguar, black, brown interior, £388 excellent condition generally, very attractive car in appearance, guaranteed; £450

LONDON CARS, 592-6, Greenford Rd., Greenford, L. Middlesex, Wexlow 2643. 17400

TOM GARNER Ltd., offer

HENLYS, Ltd.
ENGLAND'S Largest Jaguar Distributors.
SELECTION of all models at attractive prices.

DEVONSHIRE House Piccadilly, W.1. (Grosvenor 2287).
HENLY House, 385, Euston Rd., N.W.1. (Euston 4444).

DEPOTS at:—
MANCHESTER (Blackfriars 7843).
BRISTOL (Bristol 21326).

BOURNEMOUTH (Bournemouth 6314).
NORTHAMPTON (Northampton 907).

CAMBERLEY (Camberley 77).
STREATHAM (Streatham 7751).
HOUNSLOW (Hounslow 3454).

FINCHLEY (Finchley 0081).
GREAT WEST RD. (Ealing 3477). Official Jaguar Service Station.

CAMDEN TOWN SERVICE STATION (Guliver 4141).
HENLYS, Ltd., England's Leading Motor Agents. 10027

WARWICK WRIGHT, Ltd., offer:—
1949 Jaguar 3½-litre Mark V saloon, gunmetal grey, red leather, 7,000 miles; £1,850.

WARWICK WRIGHT, Ltd., 150, New Bond St., W.1. Mayfair 9761. 17619

SWANMORE CAR SALES offer:—
1949 Jaguar 3½ Mark V, H.M.V., 5,000 miles, silver.

1949 Jaguar 3½ Mark V, H.M.V., 10,000 miles, black.
1947 Jaguar 1½ saloon, black, 12,000 miles, discs.

1946 Jaguar 3½ saloon, silver, unmarked, discs.
1946 Jaguar 1½ saloon, silver, red leather. 1

EXCHANGES, terms.—Swanmore Garage, 1176, Christchurch Rd., Boscombe. Tel. Southbourne 1022. 17235

HAROLD RADFORD & Co., Ltd.,
1947 (Feb.) 3½-litre Jaguar saloon finished in black with brown leather upholstery, one private owner, genuine total mileage 19,000, just fitted with brand new engine, unused since, new tyres, radio, heater and demister, excellent condition throughout.

HAROLD RADFORD & Co., Ltd., Melton Court, South Kensington, S.W.7. Tel. Kensington 6642 (4 lines),
JOY SALMON AUTOMOBILES, Ltd., offer the following car:—

1949 Jaguar 1½-litre razor-edge saloon, 4,000 miles; £1,350.—Portsmouth Rd., Thames Ditton, Esherbrook 3551-2-3.

1948 Jaguar 2½-litre saloon, suede green, green leather, 10,000 miles.
56 Park Lane, W.1. Regent 4866.

1946 2½-litre Jaguar saloon, mechanical and coachwork condition excellent.—Below.
1948 Jaguar 1½-litre extra equipment saloon, black, brown leather, one owner, coachwork and mechanically excellent condition throughout.

1939 (Sept.) Jaguar 2½-litre saloon, black, radio, mobile, exceptional condition, taxed year; £1,050.
R. C. WIMBUSH, Ltd., 312, Earis Court Road, S.W.5. 17457

1949 Jaguar Mark V 3½-litre, grey/grey hide and covers, 4,200 miles.—R. B. Smith, Try Parli. Aldersham. 17129

1938 3½-litre S.S. Jaguar, completely reconditioned engine (10 miles), recoloured, exceptional condition.
G. Euston 3268. 13458

1948 1½-litre Jaguar saloon, metallic grey, silver/black upholstery, 11,000 miles, an excellent example of this model.
ROSTACE WATKINS, Ltd., 12, Berkeley St., W.1 (Mayfair 5951), and 12, Chelsea Manor St., S.W.3 (Fleming 8161). 17767

1948 2½-litre saloon, under 10,000 miles, exceptional condition throughout; £1,150; all replies answered.—Box 6163.
E. POWELL MOTORS, Ltd., for Jaguar cars, East London main agents, 321, Romford Rd., Forest Gate, E.7. Maryland 4818-9. 10700

1947 Mark V Jaguar 3½-litre saloon, 8,000 miles, black, excellent condition.—Gordon House, 378, Euston Rd., N.W.1. Euston 6811. 17443

1947 1½-litre Jaguar special equipment saloon, radio, immaculate condition; £895.—Broadway Motors, 67, High St., Hounslow. Tel. 0175. 17512

4000 miles.—1049 (June) Jaguar 3½-litre Mark V saloon, black.—Ernest Sutton, Cleve Hill 35 (Cheltenham) (Trade enquiries only please.) 17727

12 silver trim, H.M.V. radio, heater, Ace discs, perfect; £1,050.—Sunningdale Motors, Sunningdale, Ascot 819. 17874

1947 3½-litre Jaguar, in suede green, almost unmarked, open to any examination; £985.—Jack Rose Ltd., Stafford Rd., Wallington, Surrey. 15942

UNPRECEDENTED Jaguar bargain! £200 under list!—1947 (1210 tax) Jaguar 3½-litre S.E. saloon, genuine 17,000 miles only, exceptional condition, absolute gift. £1,075; also

1939 Jaguar 1½-litre sports saloon, in excellent condition, throughout, barge; £395. Z. Motors, 100, Palmerston Rd., Kilburn, N.W.8. Mai. 4723. 17797

1947 (May) 3½-litre Jaguar, 24,500 miles, disc wheels, one owner, perfect condition; £1,100.—B. W. Seashell, Sandringham Park, Tockley, Tockley, Tel. Tonbridge 3451. 1608

JAGUAR 3½, 100, £10 tax, 1,200 miles, complete overhaul, new tyres, hood, spare high compression head and carbs, new windscreen—Hastings, 25, Holland Villa Rd., W.14. Bay. 6193.

1947 Jaguar 3½-litre sports saloon, black with fawn leather, Ace discs, radiomobile, excellent condition; £815; terms, part exchanges.—J. Jordan, Sandy, Beds. Tel. 64. 1768

CAMDEN MOTORS—Jaguar 1½-litre 14hp motor, saloon 1939, finished black with attractive leather upholstery, excellent performance, tyres alone new; genuine value for money at £495.

CAMDEN MOTORS—Jaguar 1½-litre 14hp motor, saloon 1940, extensively overhauled mechanically, beginning of this month, specially recommended; £500.

CAMDEN MOTORS—Jaguar 2½-litre fourseater, grey with red leather, low mileage; £695.

CAMDEN MOTORS—Jaguar 2½-litre foursome and head coupe 1939, finished maroon with leather interior to match, outstanding mechanical order, fitted fully chromed Ace discs, all tyres renewed recently; £495.

CAMDEN MOTORS—Jaguar 3½-litre sportsman's saloon 1940, finished in clean black cellulose with plain upholstered upholstery, and identical features to a special equipment model, air-conditioning, built-in radio, special Lucas equipment, etc.; outstanding opportunity at £510.

CAMDEN MOTORS, Jaguar Specialists, Lake St., Leighton Buzzard, Beds. Tel. 2041 (4 lines) write for our 18-page, post free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive. 17619

JAGUAR 2½-litre saloon, 1936 model, fitted P.160, Philco radio, etc., excellent bodywork in grey, new interior, good mechanically, with new tyres; £275.—A. Byron Rd., North Wexley. Arn. 7057. 17608

1949 (Jan.) 3½-litre Jaguar saloon, black, brown leather, 5,500 miles only, radio, heater & mister, discs, immaculate condition throughout; £1,300.—Tel. Stoke-on-Trent 0437, 9-5 week-days. 17061

1947 (July) Jaguar 1½-litre special equipment saloon, black, brown leather, Duophon twin horns and badge bar, one owner, new tyres, immaculate; £875.—Hampton Park 294, Eastbourne, before 10 a.m. 1696

£475 S.S. Jaguar, 1939 drophead coupe, had extras, disc wheels, etc., leather interior, tip top performance, very attractive; many others. Benmotors, 1, Clarendon Rd., Holland Park, W.11 (9 yards Holland Park Tube). Par. 5066-7, Mon.-Sat. 6-4.

495 cns.—Jaguar 100 1939 3½-litre super sports 1½-seater, bronze with red leather, outside exhaust, bronze cylinder head, Scintilla magneto, standard and racing screens, unworn tyres, carefully used, excellent condition; terms, exchanges, list; open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hamstead Tube). Hampstead 6041. 1780

Jaguar Cars Wanted
HENLYS, Ltd.
ENGLAND'S Largest Jaguar Distributors.

DEVONSHIRE House, Piccadilly, W.1. (Grosvenor 2287).
HENLY House, 385, Euston Rd., N.W.1. (Euston 4444).

GREAT WEST ROAD (Ealing 3477). Official Jaguar Service Station.
CAMDEN TOWN SERVICE STATION (Guliver 4141).

MANCHESTER—1-5, Peter St. (Blackfriars 7843).
HENLYS, Ltd., England's Leading Motor Agents. 10027

ROWLAND SMITH'S, the Jaguar buyers.—Hamstead High St. (Hamstead Tube). Ham. 6041.
D.S.

THE Jaguar buyers.
DICKES CAR SALES, 355-401, High Rd., Kilburn, W.8. 1751-2. 1578

COOMBS & SONS (GUILDFORD), Ltd.
URGENTLY wanted, good condition, post-war Jaguar car, offers appreciated.—Folton Road, Guildford. Tel. 62907. 15139

SAUL & SLATTERY, Ltd., 44-46, Aldermans Hill, N.11. Tel. Pal. Grn. 1205-1173. 1776

MAIN dealers, urgently require modern Jaguar cars, Tel. Pal. Grn. 1205-1173. 1776

PRE-WAR 1½-litre drop head coupe required.—Tel. Tulse Hill 2357 or write Box 6196. 16980

CASH immediately for good Jaguar.—H. F. Edwards, 28, Upper High St., Epsom 9400. 17770

MARSTON MOTOR CO., Ltd., for your Jaguar.—Tel. Ma. 8000. Seven Sisters Rd., Tottenham, N.15-16.

POST-WAR Jaguar required, cash payment.—Messrs. J. H. Stretcher, Ltd., S.W.7. Tulse Hill 4683. 17480

CASH buyers of low mileage 1½-litre Jaguars; distance no object. Hattons, Lord St., Southampton. Tel. 2268. 10794

BRITISH & COLONIAL MOTORS, Ltd., require good Jaguar cars.—Upper St. Martin's Lane, W.C.2. Tem. 3598. 17474

URGENTLY required, Jaguar 1947-8 1½-litre saloon, low mileage, no dealers.—J. Ison, 94, Dilton Fields, Cambridge. 16786

PRE-WAR Jaguar cars urgently wanted for cash.—Tel. Tulse Hill Motors, Ltd., 28, Tulse Hill, S.W.2. Tulse Hill 7105. 14980

J. R. INWARDS, Ltd., main dealers for West Middlesex, anxious to buy Jaguar cars.—High St., Ruislip 3035/4-5. 1068

PRIVATE owner wishes to exchange 1½-litre Jaguar (18,000 miles) with cash adjustment, for 3½-litre Mark V.—Box 6198. 17803

GATEHOUSE are buyers of good clean Jaguar cars.—Gatehouse Motors, Ltd., Highgate Village, London, N.22. 17728

SWANMORE CAR SALES urgently require Jaguar cars.—Swanmore Garage, 1176, Christchurch Rd., Boscombe. Tel. Southbourne 1022. 17240

URGENTLY required, low mileage 1943-9 Jaguar 1½-litre saloon.—Gibsons Sports Cars (Christchurch), Ltd., Lyndhurst Rd., Christchurch, Hants. Tel. 1681-1780

Jaguar Spares and Service

HENLYS, Ltd.
ENGLAND'S Largest Jaguar Service Station.
GREAT WEST ROAD, Brentford (Ealing 3477).
SPARES and replacement engines for all models.
AND at Manchester, Chesham Hill Rd. Deansgate 6216-7.
QUICK completion of repairs. [0563]
SAUL & SLATER, Ltd., 44-46, Aldermans Hill, N.13.
Full stock of spares, Jaguar repair and maintenance.
Service Station: Green Lanes, Palmers Green, N.15. Tel. Grn. 1205-7173. [7725]
PERLESS MOTORS, Ltd., main dealers for Buckinghamshire, Jaguar spares, replacement units and main facilities. Bath Rd., Slough. Tel. 22394. [10430]
P. F. POWELL (MOTORS), Ltd., East London distributors for Jaguar cars, sales, service and spares. 321 Romford Rd., Forest Gate, E.7. Maryland 4813-9.
SMILEY COURT MOTORS SERVICE STATION.
Comprehensive range of all Jaguar spares in stock. Specialist service and maintenance for Jaguar cars. Wembley Court Motors Service Station, Forty Avenue, Wembley. Arnold 1154-5. [4105]

JEEP

JEEP car, good condition, £10 tax; £225. -Jacquier, Ltd., 225-7, Hammermith Rd., W.6. Riverside 897-3.
[1950] (first reg.) Jeeps, all types, spares, [7646]
Davies & Groves, 1-5, Dorset Close, N.W.1. [0619]
[1939] Britain's leading Jeep specialists; all spares in stock; motorcraft rebuild Jeeps; detachable bodies, utilities; 24 hour service.
MOTORCRAFT GARAGES, Station Approach, Gunpowder Square, W.1. Tel. 2241. [0241]
JEPS, right or l.h.d. range of bodywork, private or commercial. -Wick Auto, 100% Jeep Firm, Hampden Wick, Kingston-on-Thames (4718). [0843]
JEPS, Autowork, Ltd., offer several first-class Jeeps and trailers; vast stocks of Jeep spares; exchange plan. -Winchester, Tel. Winchester 4834.
METAMET's 12 Jeep versions, retain all advantages of Jeep, comfort of luxury sports car, 6 months' guarantee, over 20 m.p.s., optional commercial registration. -100b, Belisle Lane, N.W.3. [0537]
WILLIAMS Jeep, station wagon body, small mileage, all vehicle completely rebuilt this summer (all bills), pressure 60lb, really sound; for sale owner having no further need; £300. -Haig, Nutbourne Common, Fulbourn, Sussex. [7074]
UNIVERSAL CAR DISTRIBUTORS (LONDON), Ltd.
-Jeeps and Jeep spare parts, home and export, all spares stocked, exchange plan engine, gear box, water pump, etc.; new hoods, basket seats, brake linings, etc. -331-333, High Rd., Chiswick, London, W.4. Chiswick 1919. [0033]
Jeeps Wanted
ROWLAND SMITH'S, the Jeep buyers. -Hamstead High St. (Hamstead Tube). Ham. 6041. [0944]

Jeep Spares and Service

100% Jeep firm, all spares by return, or over counter, small or large quantities. -Wick Auto (see under Jeeps). [0366]
METAMET for all Jeeps, spares, exchange unit service; expert Jeep repairs; to order spares "phone Hamstead 3231-100b, Belisle Lane, N.W.3. [0539]
JEPS -Autowork (Winchester), Ltd., the largest of spare part stockists for the Jeep; everything in stock; engine exchange plan, gearboxes, axles; rust proof or write your name. -Winchester 4834 and 3406.

JENSEN

CLAND & TABOR, Ltd., offer:-
1938 Jensen saloon, just fitted with new engine, black, grey leather, exceptionally smart appearance, all new tyres, overdrive; £650.
APPLY North Road Garage, Welwyn 481. [6739]

JOWETT

CAR MART, Ltd.
JOWETT Javelin 1949 saloon, 12,000 miles; £975. -J Car Mart, Ltd., 150, Park Lane, W.1. Grosvenor 3434. [7557]
1949 Javelin saloon, in turquoise blue, with radio; £925.
C. W. WILKIN, Ltd., 1, Weston Park, Kingston-on-Thames, Kin. 2241. [7947]
JOWETT 10 1937, top overhauled, excellent condition; £220. -Gadecia, Cedar Rise, N.14. Enterprise 5255.
JOWETT and Javelin main agents, spares and specialist service. -Colliver-Fisher, Ltd., Northwood, Middx. Tel. 777 (4 lines). [7271]
MARRIOTT'S GARAGE, Worthing, leading West Sussex agents for Javelin and Bradford. Spares (including pre-war). Service, Tel. 1583. [0575]
JOWETT 8hp 4-door saloon, excellent condition J throughout, leather upholstery, radio, one owner; 4200 or near. -Rouse, 96, Gloucester Rd., Cheltenham. [1948-49] Javelin 4-door saloon, with radio, almost unmarked, red upholstery; accept £925. -Jack Rose, Ltd., Stafford Rd., Wallington, Surrey. Wallington 6577-4. [7561]
1949 Jowett Javelin, low mileage, reconditioned; Main Agents, Automobile Engineers. Telephone Brookwood 2201-2202. [7015]
COOTER & GREEN, Jowett Main Agents. -Javelin J and Bradford spares, repair, sales and service. -Eden Park Garage, 485, Upper Elmiers End Rd., Beckenham, Kent. Tel. Beckenham 2565. [0502]
1949 (June) Jowett Javelin saloon de luxe, turquoise, finest leather upholstery, heater/demister, oil filter, etc. £575. Absolutely brand new, showroom condition throughout; owner only driven, 11,000 miles, oil consumption nil, petrol 34 m.p.s.; reason for selling, illness; £550. -Hawesell, 3, Pier Avenue, Tankerton, Kent. [7144]
Jowett Cars Wanted

Jowett Cars Wanted

H BENTLEY & PARTNERS.
WISH to purchase Javelin cars. -Backville House, 40, Piccadilly, W.1. Stone 3094. [3990]
ROWLAND SMITH'S, the Jowett buyers. -Hamstead High St. (Hamstead Tube). Ham. 6041. [0945]

RAYMOND WAY, the hire-purchase specialists, are still buying Jowett, and have unlimited cash available. -Canterbury Rd., Kilburn, N.W.8. Maids Vale 6344 (10 lines). [3130]

Jowett Spares and Service

NEWNHAMS, Ltd.
JAVELIN and Bradford main agents, spares and service specialists. -Newnhams House, 235-9, Hammermith Rd. W.6. River 4666.
MILESTONES (SERVICE GARAGE), Ltd., main agents Jowett Javelin, Bradford vans and trucks. **LARGEST** stock spares in Southern England, immediate despatch trade or private. -Tel. Erith 2463, 2639, 309, Erith Rd., Bexleyheath. [0571]
JOWETT-BRADFORD. -For quick repairs or any spares try Bunting's Jowett Agency. Harrow 1906.
CROXYDON, Godfray, Ltd., for full Jowett service and comprehensive range of spares. -228-234, London Rd., Croydon, Cro. 3641. [0463]
FOR full Jowett service and comprehensive range of spares. -Moon's Motors, Ltd., Dorset House, Marylebone Rd., N.W.1. (Welbeck 7988). [19247]
KINGSTON-ON-THAMES main agents for Jowett Javelin and Bradford vans; spares and service. -G. W. Wilkin, Ltd., 1, Weston Park and 84, Eden St., Kingston 2241-4. [16418]
MOTORS, Ltd., Park Rd., Teddington, Middlesex, Tel. Kingston 0710. The Jowett specialists and main agents; over 28 years' Jowett experience; spares and service. [0759]

LAGONDA

BROOKLANDS,
LAGONDA distributors for London.

HAVE for show, demonstrations and early delivery the new 2.6-litre saloons and coupes; details and catalogue available. [1939]
Lagonda V.12 4-seater drop head coupe; examined and approved by makers. [1939]
Lagonda V.12 sports saloon, maroon, beige leather, excellent, approved by makers. [103]
New Bond St., W.1. Tel. Mayfair 8351-6. [7533]

HAROLD RADFORD & Co., Ltd.
OFFICIALLY appointed Lagonda retailers.

SALES and service at Melton Court, South Kensington, London, S.W.7. Kensington 6642 (5 lines). [0264]

THE CHESTER ENGINEERING CO., Ltd., offer:-
1937 Lagonda 4½-litre with pillarless saloon body, finished in good black with grey leather upholstery, over £700 was spent on this car in 1946, and has not been licensed since; £875. [7314]
CITY RD., Chester. Tel. Chester 23477. [6327]

1939-40 Lagonda V.12 drop head coupe, £10 tax; £1,250. -Thompson-Doxey, Ltd., Sefton St., Southport. Tel. 3936. [6327]

LAGONDA 1935 sportsman's pillarless saloon in excellent condition, price £225. -King, 38, Wake Green Rd., Birmingham 13. [7216]

3-litre tourer, beautiful condition, first reg. 1935, one of the best of this type about; £300; private sale, but terms can be arranged. -Box 6180. [6774]

£150 -Lagonda 5-litre, selector, 1935, excellent condition and good black with grey leather upholstery. -Longmore Close, Liverpool 10. -Ain. 2973. [7158]

LAGONDA special, fitted Perkins diesel engine, reconditioned, 35 m.p.s., £10 tax, registered 1950, perfect. -£750; payments. -Vaughan, 17, Astwood Mews, S.W.7. Tel. 3139. [7695]

DAVIES MOTORS, Ltd. (Managing Director, J. E. Davies, 20 years' service manager to Lagonda, Ltd.) offer an excellent selection of used Lagonda cars; may be furnished details. [1938-9]
WE are open to purchase a few late models for rebuilding. [273]
London Rd., Staines. Tel. 3457-8 or (private) Walton 1562. [0287]

4½-litre Lagonda drop head coupe, registered July 1936, recent check at Lagonda; a very beautiful car, in first-class mechanical condition; £695. -Motor House, 1, Shoulton, Worcester. Tel. Peckham 275. [6667]

LAGONDA V.12 d.h. coupe, March, 1939, Sanction 2 engine gear box and back axle, maroon with maroon leather upholstery, Newton shock absorbers, undoubtedly the finest specimen in the country; offers to R. MARTINDALE, Ltd., 14, Harrison St., Brighton, Leade, 13. [7522]

ROSE & YOUNG, Ltd., offer 1938 Lagonda V.12 4-door saloon, black, green leather, exceptional condition, and good black and chrome, with fine quality leather upholstery and interior furnishings which are positively in superb condition; the whole car is practically without flaw and can literally be compared with a model which has had, at the most, only a few months' use; the log book states that a new engine was fitted in February, 1949, the mileage since replacement being of nominal figure; naturally, therefore, mechanical condition of the Lagonda is quite equivalent to its magnificent appearance, scrupulous care and attention have been lavished on its upkeep entirely regardless of cost, performance is without equal, and it is a real credit to the engineering of this car. -See below. [1941]
Streattham Hill, S.W.2 (1 minute Streattham Hill Station).
Tulse Hill 6464. [7660]

£1195 -Lagonda V.12 short chassis 4-light sportsman's saloon June, 1938, immaculate black and chrome, with finest quality leather upholstery and interior furnishings which are positively in superb condition; the whole car is practically without flaw and can literally be compared with a model which has had, at the most, only a few months' use; the log book states that a new engine was fitted in February, 1949, the mileage since replacement being of nominal figure; naturally, therefore, mechanical condition of the Lagonda is quite equivalent to its magnificent appearance, scrupulous care and attention have been lavished on its upkeep entirely regardless of cost, performance is without equal, and it is a real credit to the engineering of this car. -See below. [1941]
Streattham Hill, S.W.2 (1 minute Streattham Hill Station).
Tulse Hill 6464. [7660]

£545 -Scarce and highly desirable Lagonda Rapid 4½-litre 4-seater sportsman's Roadster 1935/6, a classic example of this famous and much sought after marque, immaculately maintained by enthusiastic specialist; maker's coachwork beautifully finished in cream with blue trim, upholstered in spotless blue leather, several special features and instruments; the general mechanical condition of this particular Rapid leaves little or nothing to be desired, the engine is in most outstanding condition, performance, gear shifting, braking and steering will delight even the most exacting fanatic, a brand new hood and all-weather equipment together with all Dunlop tyres, also practically brand new, contribute to make this Lagonda a genuine and unrepeatable bargain at the price; offered with a comprehensive written guarantee by
CAMDEN MOTORS, Leake St., Leighton Buzzard, Beds. C. Tel. 2041/2/3. Write for our 18-page post-free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire-purchase facilities; part-exchanges; free delivery; showrooms open till 8 p.m., Monday-Saturday inclusive. [7853]

1939 March registration, Lagonda V.12 short chassis, fitted saloon body, works history available; the whole car in first-rate order; a unique opportunity at £1,295. -Bells Service Garages, 144, London Rd., Kingston-on-Thames. Kingston 1185.

V.12 short chassis Lagonda drop head coupe, the model designed and built by W. O. Bentley; has been run 14 months only, and a genuine 14,000 miles, original tyres, spare wheel unused, finished in lavender grey with maroon leather interior, and is without a doubt the finest specimen of this famous marque available today, maximum speed well in excess of 100 miles an hour, comparatively economical, first registered 1939, one private owner from new; photograph available to genuinely interested purchaser; other car considered in part exchange. -Whitehall, 20, Leamington Rd., Coventry. Tel. Coventry 68223. [16987]

Lagonda Cars Wanted

P.C.E.
THE Lagonda buyers.

DICKIE CAR SALES, 585-601, High Rd., Kilburn, N.W.6. Maids Vale 6888-9. [15727]

ROWLAND SMITH'S, the Lagonda buyers. -Hamstead High St. (Hamstead Tube). Ham. 6041. [0945]

PRIVATE purchaser requires Lagonda coupe 1938-40. -Particulars to G. R. Remfrey, 17A, Albert Ave., Hull. [7149]

LAGONDA cars. -A limited number of home-made orders now acceptable; used models always wanted. -Lagonda House, 7-9, Russell Parade, N.W.11. Speedwell 0011 (ten lines). [0285]

Lagonda Spares and Service

LAGONDA owners are advised to contact the manufacturers for service, overhaul and spares for the 4½-litre and V.12 models; service engines in stock. -L. A. G. 334, Service Department, Victoria Rd., Feltham, Middlesex. Tel. Feltham 2231. [10904]

DAVIES MOTORS (Managing Director, J. E. Davies, 20 years' service manager to Lagonda, Ltd.) are concentrating on the servicing and rejuvenation of pre-war Lagondas; we have revolutionized the ride of early models and we now offer a reliable conversion on pre-war cars with L.A. [0217]

273 Walton 1562. We are open on Saturday mornings. [0217]

LANCHESTER

CAR MART, Ltd., London Distributors.

LANCHESTER 14 1949 Roadster de luxe saloon, 6 months' guarantee. -£875. -Car Mart, Ltd., Euston Rd., N.W.1. Euston 212. [7556]

STRATSTONE, Ltd., Lanchester specialists offer:-
1939 Lanchester 15 saloon, black with brown leather, one owner, chauffeur kept; excellent condition; £795. -40 Berkeley St., W.1 (Mayfair 4404). [7291]

1938 Lanchester Roadster, 13hp, superb condition, cellulosed blue; £445. [7291]

L. SERVICE STATION, Kingston Vale, S.W.15. [0545]

LANCHESTER 10 saloon May, 1947, very nice condition, blue cellulose and upholstery, fitted heater, taxed. [7241]

E. CLAYBURN & Co., Ltd., 57, Hall Gate, Doncaster. Tel. Doncaster 3414. [7241]

S. PINK (BOURNEMOUTH), Ltd., engaged exclusively in the distribution of Daimler and Lanchester cars, consignment buying or selling; all spares and every service. [7545]

DAIMLER House, Bournemouth, Tel. 5405. [0545]

1938-9 Lanchester drop head coupe by Wingham, low mileage, well-kept car fitted new hood; £575. -Bells Service Garages, 144, London Rd., Kingston-on-Thames 1185. [7968]

1937 Lanchester 14hp Roadster saloon in black with brown upholstery, radio fitted and in very nice condition throughout; £295; 3 months' guarantee. -Tankard & Smith, Ltd., 97, Peckham Rd., S.E.15. Rodney 2051. [6699]

Lanchester Cars Wanted

C. M.
THE CAR MART, Ltd. London distributors, wish to purchase Lanchester cars. -150, Park Lane, W.1. Grosvenor 3434. [7557]

LONDON CARS require good used 11/14hp Lanchester 15 saloons, later models preferred. [14108]

LONDON CARS 562-6, Greenford Rd., Greenford, W.10. [7241]

ROWLAND SMITH'S, the Lanchester buyers. -Hamstead High St. (Hamstead Tube). Ham. 6041. [0945]

CASH buyers of low-mileage Lanchester 10s; distance no object. -Huttons, Lord St., Southport. Tel. 2268. [7291]

BRITISH & COLONIAL MOTORS, Ltd., require good Lanchester cars. -Upper St. Martin's Lane, W.C.2. Tem. 3588. [7472]

Lanchester Spares and Service

A ROOT MOTORS, Ltd. -Preselector gear boxes; exchange and repairs. -169, Fulham Rd., S. W.14. Kensington 7501. [0237]

LANCIA

1941 (first reg. 1945) Lancia Astura 27hp 4-door saloon by Farina, blue with Bedford cord upholstery exceptional condition throughout. [1941]
New Bond St., W.1. Tel. Mayfair 8351-6. [7534]

JOHN S. TRUSCOTT, Ltd., for Lancia cars.

ONLY first-class examples are offered.

APRILIA and Augusta models available.

EXCHANGES, deferred terms. -173, Westbourne Grove, W.11. Bay. 4274. [7418]

£465 -1937 Aprilia, maintained by Lancia's; fitted large engine and all modifications; Ekco radio; 4 new tyres; paint-work dull. [7417]

THE FORGE GARAGE (PETERSHAM), Ltd., 192, Petersham Rd., Petersham, Surrey. [7417]

£260 -1935 Lancia Augusta, two owners only, 32 m.p.s., oil negligible tyres in good condition. -Grant, Cedars, London Rd., Shrewsbury. [7152]

LANCIA Aprilia 1939 drop head cabriolet, Farina body, excellent condition and performance, owned and maintained by Lancia enthusiast; £750. -Tel. St. Albans (Bristol) 3328. [7190]

LAMBDA 8th Series Weymann saloon, £90 Laytall overhaul in 1948; £150, 50, Brighton Rd., Bantled, Surrey, Burgh Heath 1550. [7061]

1936 Lancia Augusta, fitted with Continental type streamline saloon body, and recently fitted with reconditioned engine; £425.—Peter Bantock Car Sales, 104, High Rd., Chiswick, W.4. Chiswick 375/5670. [5715]

1938 Aprilia, very good condition generally, negligible rust in chassis; interior recently completely overhauled, new carpets, linings, seats re-covered; resprayed 6 months ago; can be seen Colchester by appointment; no dealers; £525.—Box 6157. [4812]

CERTAINLY the most perfect Aprilia in G.B., one owner, never damaged, only 5,400 miles since complete overhaul, including 1949 modifications throughout; the following are ready for 50,000 miles safe, fast, road work, converted front suspension, complete engine check, 1½-litre, clutch, gearbox, transmission, couplings, brakes perfect, Android rear, sliding roof, two spare wheels, seat covers, many spares, new leather, chrome cellulose, tyres, interior trimming; photos on request; £975 will give perfect car, 30 m.p.g., 80-85 mph with no skids, breakdowns. Private owner.—Box 6421. [7576]

Lancia Cars Wanted

LANCIA Aprilia wanted any year.—F. P. Breen, High Rd., N. 30. Tel. Hillside 2393. [3943]

JOHN S. TRUSCOTT, Ltd., urgently require first-class Lancias.—173, Westbourne Grove, W.11. Bay 4274. [7434]

KEVILL DAVIES & MARCH, Ltd., will buy second-hand Lancia Aprilia—41-42, Hays Mews, Berkeley Sq., W.1. Gros. 2563. [0508]

LANCIA Aprilia, latest models wanted.—A. Freeman, Ltd., Grosvenor Garage, Burnage Lane, Manchester 19, Rus. 2874-5. [10077]

Lancia Spares and Service

LANCIA (ENGLAND), Ltd., Ealing Rd., Alporton, Wembley 3, 26561.—Factory guaranteed spares and repairs; 40-hour service for reconditioned components, engines, dynamos, starters, etc., etc. [0000]

LEA-FRANCIS

TOM GARNER Ltd., offer

1948 Lea-Francis 14hp sports 2-seater, maroon with maroon leather, 4,000 miles. [7536]

TOM GARNER Ltd., 10-12, Peter St., Manchester, 2, Blackfriars 9265-6. [7536]

CHARLES FOLLETT, Ltd., Sole Distributors London and Home Counties, offer:—

1949 Lea-Francis Utility I.F.S., 1 owner, 15,000 miles, excellent condition and completely checked over; £815. [0000]

1949 Lea-Francis 2½-litre (18hp) streamlined saloon, I.F.S., maroon, beige leather, radio, heater, fog lamps, petrol pump, rimblechens, etc.—this is a demonstration car and therefore very carefully maintained; £1,650. [0000]

1949 Lea-Francis 14hp 4-door saloon, I.F.S., chauffeur maintained; £1,125. [0000]

1948 Lea-Francis 14hp saloon, black, brown leather, 18,200 miles, H.M.V. radio, heater, car has just been overhauled by makers, guaranteed; £950. [0000]

18 Berkeley St., W.1. May. 6266. [0000]

OFFICIAL Lea-Francis London Service Station, 12, Wellesley Ave., W.6. Riv. 1413. [7931]

Lea-Francis Cars Wanted

CHARLES FOLLETT, Ltd.—Lea-Francis distributors for London and the Home Counties, are always glad to hear from Lea-Francis owners.

SHOWROOMS: 18, Berkeley St., W.1. May. 6266. [0000]

OFFICIAL Lea-Francis London Service Station, Works and Stores:—

12 Wellesley Ave., W.6. Riv. 1413. [8963]

Lea-Francis Spares and Service

LEA-FRANCIS CARS, Ltd.

SPARES and service for all models from the manufacturers, Read, Office and Works: Much Park, Coventry, Tel. 50294-6. [0392]

CHARLES FOLLETT, Ltd., sole distributors for Home Counties, Bucks and Sussex.

SHOWROOMS: 18, Berkeley St., W.1. May. 6266. [0000]

SPARE parts

SERVICE: 12, Wellesley Ave., W.6. Riv. 1413. [8970]

LINCOLN

JOE THOMPSON (MOTORS), Ltd., offers:—

1948 Lincoln Continental saloon, colour maroon, one of America's most expensive cars costing approximately 5,000 dollars. [0000]

JOE THOMPSON (MOTORS), Ltd., 97, Fulham Rd., South Kensington (next to Michellins). Ken. 4858. [0000]

1750 gns.—Lincoln V12 (reg. March, 1947, £10 tax), 40hp V-twelve, R.H.D., specially imported American 4-door sedan with winding division, black, grey cloth upholstery, steering column gear change, electrically operated windows, heater, unworn tyres, small mileage, one careful owner, superlative condition; terms, exchanges; list, open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). Hampstead 6041. [7941]

Lincoln Cars Wanted

SIMPSON'S MOTORS (WEMBLEY), Ltd., urgently require all models Lincoln.—Wembley 3503 [7019]

LINCOLN-ZEPHYR

1939 Lincoln-Zephyr V12 4-door saloon, black with cloth upholstery, which is unmarked. A really magnificent specimen and has been maintained by Lincoln Cars and is faultless, new set of tyres and non-blow-out inner tubes at a cost of £80; this car will, we are sure, be sold to the first caller; £750; taxed to December.—Dalwood Motors, Finner Green, Finner 670. [8022]

LLOYD

DICKS CAR SALES offer:—

1949 Lloyd sports tourer, 5,000 miles only, definitely as new; £450. [7975]

DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maida Vale 6888-9. [7975]

LOYD 650 sports 4-str. 1949 red, perfect condition, 16,500 miles, new set of tyres, £625. [0000]

1949 (Auz. Lloyd model 650 sports Roadster 6hp, new condition throughout, extras, small mileage, cost £500; 1 only offered at £445, immediate delivery. [0000]

GUY ALFREDE & Co., Ltd., 6-7, Warren St., W.1. Euston 3268. [7877]

MERCEDES-BENZ

(GREAT BRITAIN), Ltd.

1939 type 540K 38hp supercharged 4-5-seater drop head, black, with maroon leather upholstery, engine recently overhauled by us; £1,275.—Victoria 8715. [0000]

VARIOUS Mercedes-Benz models for sale. Also interested in buying.

CHIPPSTEAD MOTORS, The Onslow Garage, 107, Fulham Rd., London, S.W.3. Flaxman 0052. [7825]

MERCEDES-BENZ Spares and Service

MERCEDES-BENZ (GREAT BRITAIN), Ltd., Sales Service and Spares, 111, Grosvenor Rd., S.W.1. Victoria 8715-6. Night Service: Victoria 3144. [6530]

ELITE MOTORS offer

1937 M.G. T.A. 2-seater, completely recoloured in black, red leather upholstery, excellent performance; very good all-weather equipment. £565. [0000]

1933 M.G. J.2 2-seater, finished in British racing green, an exceptionally good example of a popular sports car, fitted with hood and new side-screens; £189. [0000]

ELITE MOTORS, 951-961, Garratt Lane, Tooting Broadway, Tel. Balham 2474 (four lines). [7665]

WIDEBAY CARS offer:—

£195—M.G. Magnette 12hp 4-seater, goes well, excellent weatherproof equipment, series N.A. [0000]

£155—M.G. Magna 12hp open 4-seater, very smart, maroon, weatherproof and sound throughout. [0000]

£95—M.G. Midget 8hp sports 2-seater, a really beautiful little runner; another at £80. [0000]

MANY other M.G.s in stock.—Woodland Car, Eton Garages, Eton Ave., Swiss Cottage, N.W.3. [7576]

WARWICK WRIGHT, Ltd., offer:—

1948 M.G. 1½-litre saloon, black, beige leather, 9,000 miles; £995. [7620]

WARWICK WRIGHT, Ltd., 150, New Bond St., W.1. Mayfair 9761. [7620]

M.G. 1939 saloon 18hp very good condition; £550.—Reading 72985. [6894]

M.G. T.A. 1937, recent overhaul, excellent condition; £340.—Tel. Pal. 5168. [7217]

£185—M.G. 1½-litre saloon, very clean.—Crappert, Fussy St., Tel. Oxford 4400. [6917]

BEARTS, of Kingston, M.G. specialists, sales, spares, repairs, 102, London Rd., Kingston, Tel. Kin. 3548. [0000]

1948 M.G. T.C. 2-seater, 18,000 miles, excellent condition throughout; offers over £600.—Box 6420. [7515]

1938 M.G. T.A. good condition, recently sprayed new seats, complete hood and acreage. £300.—Box 6300. [7039]

PERFORMANCE CARS, of Daleham Mews, Beilzie Lane, N.W.5 (Hampstead 1111) offer with 3 months written guarantee:—

1937 T.A. 2-seater; £340; 1940 T.B.11hp 2-seater; £400; 1939 N.3 Magnette; £300; 1938 M.12hp 2-seater; £295; 1931 M.12hp 2-seater; £115; a constantly changing selection of M.G.s cars available at all times. [8017]

1935 M.G. 1½-litre saloon, black, beige leather, trim and appearance; £200 or offer.—Hall, 8, Church Rd., Farnborough, Hants. [6254]

1940 M.G. 2-litre drop head coupe, reconditioned, many extras, 1940 N.3 Magnette, 1940 M.12hp 2-seater; £295; 1931 M.12hp 2-seater; £115; a constantly changing selection of M.G.s cars available at all times. [8017]

£295—M.G. Magnette open 2-seater, just completed, recently overhauled and recoloured, new hood, tonneau cover, car really as new. [0000]

BRIAN FINGLASS, Bugatti Sales & Service, 2, Pembroke Mews, W.11. Baywater 3951. After 6. [7556]

SUPERCHARGED 1934 J4 Midget, black/green, exceptional condition; £375.—H. A. Saunders, Ltd., 336/330, Euston Rd., W.1. Euston 4515. [0000]

1949 M.G. T.C. 2-seater, 4,000 miles only, luggage carrier and tonneau cover.—Ripco, Ltd., 16, Albemarle St., W.1. Regent 2952. [7638]

1948 M.G. 1½-litre saloon, black, beige leather, trim and appearance; £200 or offer.—Hall, 8, Church Rd., Farnborough, Hants. [6254]

1947 M.G. T.C. 2-seater, black red upholstery, exceptionally nice condition; £575.—Peter Guest, Ltd., Garwood, Kings Lynn, Tel. 412. [0000]

1947 M.G. 18/80 M.K. open 4-seater, rebuilt and registered in 1948, good condition throughout; £225 or best offer.—Hammersley, 15, Tower St., Richmond, Yorks. [6240]

1933 M.G. J. Model 4-seater, a really good sports car, tyres practically new; £185; seen Surrey.—Write Box 204, Smiths, 100, Fleet St., London, E.C.4. (Sept.) Tel. M.G. T.C. special model, 5,000 miles, bumpers, immaculate appearance; £725.—Herbert, 23, Earls Croft, Chislehurst, Kent. [0000]

1938 (July) 1½-litre V.A. tourer, exceptional condition, recoloured, reconditioned throughout, new hood, side-screens, Rotoflex; nearest £390.—Box 6376. [7183]

275 gns.—M.G. Midget 1936 9hp PB Airline coupe, maguro and red sliding head, red leather, very good condition; terms, exchanges.—Rowland Smith, below. [0000]

195 gns.—M.G. Midget, 1934 model, 8hp J2 2-seater, green, black leather, good tyres, very good condition, taxed; terms, exchanges; list, open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). Hampstead 6041. [7941]

P.A. type M.G. in first-class condition, very smart and sporty appearance, recently resprayed metallic green and additional £60 plus just spent on her; £614. [0000]

1946 T.C., green, in good condition, new tyres, battery, side-screens, one owner; £495.—Morgans Garage, Field Heath Rd., Hillingdon, Mids. [6510]

£225—M.G. Magnette open 4-seater, 1934, engine overhauled and maintained in absolutely excellent condition.—Park Lane House, White City, Peterborough. [6820]

£395—M.G. 18hp 2-litre sports saloon, black, very clean condition; also drop head four-seater at same price.—Bry Motters, 180-184, West End Lane, N.W.6. Hampstead 6490. [8042]

WALTER SCOTT, Ltd.—1937 M.G. T.A. 2-seater, black, excepting throughout; £345; terms, exchanges.—39, College Crescent, Hampstead, N.W.3 (Swiss Cottage Tube). Tel. 5914. [7463]

1939 M.G. T.A. 2-seater, Abingdon job, 34,000 miles, only, excellent condition. [0000]

throughout, taxed year, sale domestic reasons; £425.—104, Stag Lane, Edgware, Middx. After 6.30 p.m. [0000]

£145—M.G. Midget 8hp 4-seater sports produced in 1932, one of the few 4-seaters produced in the midget class, goes like a bomb, enthusiasts should miss this one. See below. [0000]

£275—1935—M.G. Midget series P.A. 8hp roadster, finished cream and black with black leather, very attractive looking motor, late property of experienced enthusiast-mechanic, who has made M.G. particularly his hobby, and has personally serviced and maintained engine in scrupulous fashion, special instruments and gauges are fitted, hood in good condition, side screen missing. See below. [0000]

£515—M.G. Midget series T.C. roadster 1940, since new, immaculate little car, finished black leather, taxed half rate considerable period, nominal mileage, genuine value for money. [0000]

CAMDEN MOTORS, Ltd., Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Write for our 18-page, part free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Sat. inclusive. [7000]

£495—(April, 1939), smart looking car, finished black with red leather, original cellulose, neat and tidy upholstery, outstanding runner, strongly recommended at the price. See below. [0000]

£395—Unrepeatable bargain, M.G. 2.6-litre 4-door sports model 1939, finished in original black cellulose with black leather and an abundance of chromed frontal accessories, twin trumpet horns, Noak, etc., etc.; whole car in very clean condition indeed, positively bird-proof performance, tyres like new, spares new, been on road, offered with three months' written guarantee by—

CAMDEN MOTORS, Ltd., Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Write for our 18-page, part free catalogue of nearly 400 fully guaranteed used cars; easy and confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Sat. inclusive. [7000]

T.V.A. M.G. drop head coupe, body by Tickford, 2nd registered Nov. 10, 1938, engine reconditioned, Wellworth pistons and rings, brakes relined, new shock absorbers, re-roofed, resprayed; price £250, or hire purchase for good saloon up to 14hp.—Tel. Ewbury, Surrey, 120. [7700]

M.G. K Magnette open four-seater, 1934, complete overhaul in the last 12 months, bills shown, bodywork, weathering, complete set of tyres very good, very fast and economical, sold with three months' written guarantee, taxed; £250 or hire purchase.—Noel Roscoe, Amersham Road, Chalfont St. Giles. Tel. 152. [7000]

1949 (April) M.G. T.C. Midget, 10,000 miles, black, taxed, black, red upholstery, carefully preserved, 600x16in rear wheels, lifeguard tubes, M.G. bumpers, twin Windstone horns, mirror, spot lamps, new sunshade, plate lamps, lever arm, luggage carrier, Badge bar. Overall omeau cover, sports coil, plug, compass, etc., over £100 in extras; £775.—McFall, 8, Mab Lane, Liverpool, 12. [7220]

GOOD home wanted for M-type Midget; reconditioned big-end, rebored, crank, reground, big-end, re-metalled, new mains fitted, etc.; only needs careful running-in, 4 practically new tyres, good battery, dynamo overhauled, new coachbuilt body recently fitted and centred in new colour, an outstanding little car that must be seen and tried to be appreciated; nearest £175.—Nielsen, 5, Hawes Rd., Bromley, Kent (2 mins Bromley North Station). [7690]

M.G. Cars Wanted

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R

ROWLAND SMITH'S, the M.G. buyers, Hampstead High St. (Hampstead Tube). Ham. 604. [0940]

D.C.S.

THE M.G. buyers.

DICKS CAR MART, Ltd., with to purchase M.G. cars.—320, Euston Rd., N.W.1. Euston 1212. [0908]

SLOCOMBES OF NEASDEN.

SLOCOMBES—Good clean M.G.s wanted, must be first-class condition.—268, Neasden Lane, N.W.10. [7640]

CIVIL SHEPARD requires:—

M.G. 1½ saloons—102, King's Rd., Reading 2712. [0000]

1½-litre M.G. saloon wanted, low mileage, black with green upholstery.—Details and price to private advertiser, Box 6295. [7905]

1½-litre M.G. saloon or drop head coupe, 1939-40, wanted by private buyer.—Parker, 41, Downy Rd., Erdington, Birmingham. [7004]

A PROACH us first before disposing of your M.G. car. Tanks, 42 Smith, Ltd., 194/196, Kings Rd., S.W.3. Flaxman 4807/2-3. [6239]

URGENTLY required, low mileage 1948-9 M.O. 14. [0000]

MAYFAIR GARAGES, Ltd.—Baldern St. (opposite the selfridges) 11, Mayfair 5104. Particulars want J4, P4 and T4 for cash. Phone or write for buyers to call. [7447]

RAYMOND want the hire purchase specialists, also buying M.G.s and have unlimited stock available.—Canterbury Rd., Kilburn, N.W.6. Maida Vale 6044 (10 lines). [1561]

M.G. Spares and Service

THE sole London distributors for M.G. cars

UNIVERSITY MOTORS, Ltd., 7, Herford St., London, W.1. Tel. Grosvenor 4141. [7056]

FOR M.G. mudguards, running boards, 1935-1939. [0000]

F Brooks, 85, Queens Rd., Brighton. [0000]

M.G. spares—Vertical drives, rockers, valves, and springs, from aprons, rear tanks, 12 windscreens, prompt and courteous service; see P. & A. column. [0000]

DERRINGTON, 159, London Rd., Kingston 5621-2. [1841]

M.G. engine, axle, gearbox, reconditioning, recolour, change bushes, cranks, rockers, v/drive, dynos, etc.; new rocker shafts, bushes, valves, guides, springs, sockets; wire wheels supplied and repaired, road springs new and reconditioned.—G. W. 19, Liberty, Queens Rd., Wimbledon (station). S.W.19. Liberty 3068. [0463]

TOULMIN MOTORS specialise in M.G. and M.G. cars only; repairs and complete overhauls all models. Recommended engines in stock for types P. J. T. and L. and S. Magnette; exchange service dynamo, starters, headlights with rods, gear boxes, brake shoes, vertical valves, V.D. sleeves, rockers, rocker bushes, rocker shafts, push rods, springs and gaskets set with full range of M.G. spares always available; we specialise in racing spares.

WATTS or 'Phone Toulmin Motors, The Roundabout, Hanworth, Middlesex. Tel. Molesey 583. [0349]

MORGAN
1938 Morgan 4-4, good condition: £300.-132b, Brixton Hill, S.W.2. Tulsa Hill 7304. [7219]
1945 or nearest, 1947 Morgan 4-4 2-4-seater, cream and black Greenaway, 25 St. Peter Rise, Headley Park Bristol, 3. [7064]

MORGAN 4/4, late 1938, exceptional appearance, performance, £165 makers' overhaul Feb. 1949, any reasonable trial; £355 or nearest offer.—Box 6416. [7513]
NAYLOR & ROOT, Ltd.—1949 Morgan 4/4 sports 2-seater, dark blue, beautifully maintained throughout, excellent performance, spare tyres unused, £645; 12 months' guarantee; choice of 250 quality cars; demonstrations free within 100 miles; terms available. S. East Hill, Clapham Junction, S.W.19. Bait. 5272. Open 9-6 each week including Saturday. [7953]

Morgan Cars Wanted

ROWLAND SMITH'S, the Morris 8 buyers.—Hampstead High St. (Hampstead Tube). Ham. 6041. [0949]

RAYMOND WAY, the hire purchase specialists are still buying Morgan, and have unlimited cash available.—Canterbury Rd., Kilburn, N.W.6. Maids Vale 6044 (10 lines). [5131]

MORGAN Spares and Service
1948 4/4 official spare parts stockist; service and repairs.—East Hill, 161, St. Fortians Rd., W. Langham 7733. [0514]

MORGANS—All available spares in stock.—P. H. Douglas, Morgan Specialist, 14, South Ealing Rd., Ealing, W.5. Eal. 5070. [0728]

MORRIS MINOR

1949 Morris Minor, finished maroon, under 1,000 miles.—Hippo, Ltd., 16, Albemarle St., W.1. Bait. 2952. [7844]

1949 Morris Minor saloon, 5,000 miles.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tern. 3588. [7485]

GORDON CARS (LONDON), Ltd.—1949 Morris Minor 3-door, grey, 7,000 miles.—Gordon House, 373, Euston Rd., N.W.1. Euston 6611. [7444]

MORRIS MINOR saloon, 3,000 miles, maroon, fawn leather, owner driver; £750.—N. H. Newey, 32, Bedford Rd., West Kensington, Nottingham. [7435]

1949 (Sept.) Morris Minor Tourer, 7,000 miles, black/beige upholstery; nearest £700.—Mrs. Turner, 5, Park Drive, Ilford, E.1. 2073. [7071]

MORRIS MINOR tourer, August, 1949, low mileage, in colour grey, large capacity battery, car radio, demers, twin wind-up horns, electric clock and many accessories. £750.—P. C. Hayward & Son, New St., Ashford, Kent. A. Tel. Ashford 33. [7187]

1949 Morris Minor saloon, grey, beige upholstery, under 11,000 miles, very carefully used, best security.—Hall, Station Road, Kirby Muxloe, Leicestershire. Tel. 344. [7155]

1949 (Aug.) Morris Minor saloon, grey, good condition, 5,000 miles, as new; £750.—Gibson's Sports Cars (Christchurch), Ltd., Lyndhurst Rd., Christchurch, Hants. Tel. 1681. [7304]

Morris Minor Cars Wanted

CAR MART, Ltd., wish to purchase Morris Minor cars.—320 Euston Rd., N.W.1. Euston 1212. [0716]

MORRIS MINOR wanted, in good condition.—G. S. Smith, 302, King St., Hammer Smith, W.1. Riverside 2881. [8012]

MORRIS EIGHT

J. CORYTON, Ltd.
1948 Morris 8 4-door saloon, green-black with brown upholstery, exceptionally clean and well maintained.—149, Fulham Rd., South Kensington. Ken. 1410. [7661]

1947 Morris 8 2-door saloon, black, very clean and well maintained.—149, Fulham Rd., South Kensington. Ken. 1410. [7663]

S. G. SMITH (MOTORS), Ltd., offer:—

1946 Morris 8 two-door de luxe saloon, exceptional condition, fitted radio; £465.—S. G. Smith (Motors), Ltd., 13-19 East Dulwich Rd., S.E.22. New 4444. [5294]

S. G. SMITH (MOTORS), Ltd., offer:—

1948 Morris 8 4-door, fitted radio, mileage 15,000, black with brown leather, exceptional condition; £595.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1936 Morris 8 2-seater, very sound condition; £185.—Below.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1940 Morris 8 saloon, black/blue upholstery; £185.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1940 Morris 8 4-door saloon, black/brown leather, taxed; £395.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1948 Morris 8 saloon, black, speedometer 16,000, 4-door, in showroom condition throughout; £625. Also.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1938 Morris 8 saloon, 2-door, in good condition; £325.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1938 Morris 8 saloon, 2-door, in good condition; £325.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1938 Morris 8 saloon, 2-door, in good condition; £325.—S. G. Smith (Motors), Ltd., 13-19, East Dulwich Rd., S.E.22. New 4444. [6714]

1948 Morris 8 4-door saloon, black, 8,000 miles; £675.—L. P. Dove, Ltd., 111-115, Addiscombe Rd., Croydon. Addiscombe 3084. [7394]

1935 Morris 8 2-door de luxe saloon, nice condition, taxed.—Bray Motors, 180-184, West End Lane, N.W.6. Hampstead. [7259]

1939 Morris 8 saloon, moderate mileage, good condition throughout; £375.—Lyne, Frank & Wagstaff, Ltd., 3-5, Crouch End Hill, N.8. Mountview 4401. [8025]

1947 Morris 8 4-door saloon, black and brown, one owner driver, mechanically and otherwise perfect, good tyres, taxed to end of year; £320.—Box 8395. [7159]

1939 Morris 8 4-door de luxe saloon, black, brown leather, taxed for year; £395.—K.L.M. Motors, Ltd., 101 Brighton Rd., Coulsdon, Surrey. Tel. Uplands 4841. [2939]

1945 or by instalments; Series I Morris 8hp sunshade saloon, 3 months' written guarantee; £155.—Stuart Wilson, 355, Finchley Rd., London, N.W.3. Hampstead 5712 and 5532. [7731]

1947 (July) Morris 8, 9,000 miles, one owner, black with blue upholstery, very nice throughout; £545; taxed year. East Hoxley 5136. [7138]

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1947 (July) Morris 8, 9,000 miles, one owner, black with blue upholstery, very nice throughout; £545; taxed year. East Hoxley 5136. [7138]

1936 Morris 12, in very nice condition throughout, taxed December; £259.—JACK WILLIAMS MOTORS, Ltd., 169, Priory Rd., Hornsey. Mountview 5229 and 5774. [7257]

1936 Morris 14 Series 11 sunshade saloon, original black cellulose, red leather, in genuine condition; £225.—A. BEEY AUTOS, rear of 44-46, Chase Side, Southgate, N.14 (near Tube). Palmers Green 4540. [7691]

Morris Twelve Cars Wanted
ROWLAND SMITH'S, the Morris 12 buyers.—Hampstead High St. (Hampstead Tube). Ham. 6041. [0949]

RAYMOND WAY, the hire purchase specialists are still buying pre-war Morris 12s and have unlimited cash available.—Canterbury Rd., Kilburn, N.W.6. Maids Vale 6044 (10 lines). [5043]

MORRIS FOURTEEN
Morris 14 saloon, £225; or 30 monthly instalments of £17/2/10.—Cooden Eng. Co., Bexhill, Tel. Cooden 600. [7276]

1937 model Morris saloon, completely overhauled, excellent condition; offer over £300.—721, Wel. 2024 before 7 p.m. Box 6329. [7131]

1937 Morris 14 streamlined de luxe saloon, special offer.—Bray Motors, 180-184, West End Lane, N.W.6. Hampstead 5490. [8040]

MORRIS TWENTY
95 gns.—Morris 20, 1935 model, de luxe 4-door saloon, dark blue, sliding head, blue leather, very good condition, taxed; terms, exchanges; list; open 9-7 weekdays and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). Hampstead 6041. [7923]

MORRIS OXFORD
CAR MART, Ltd.
MORRIS OXFORD 1949 saloon, 5,000 miles; £1,065.—Car Mart, Ltd., 320, Euston Rd., N.W.1. Euston 1212. [7590]

MANN EGERTON & Co., Ltd., offer:—

1949 Morris Oxford saloon, black with beige leather upholstery, 9,000 miles, immaculate condition throughout; £995.—Berkeley St., London. W.1. Regent 2073. [7431]

1949 Morris Oxford saloon, grey, beige upholstery, 11,000 miles, as new; £1,195.—Berkeley St., London. W.1. Regent 2073. [7431]

1949 Morris Oxford saloon, grey, beige upholstery, 11,000 miles, as new; £1,195.—Berkeley St., London. W.1. Regent 2073. [7431]

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1949 Morris Oxford saloon, grey, beige upholstery, 11,000 miles, as new; £1,195.—Berkeley St., London. W.1. Regent 2073. [7431]

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1936 Morris 12, in very nice condition throughout, taxed December; £259.—JACK WILLIAMS MOTORS, Ltd., 169, Priory Rd., Hornsey. Mountview 5229 and 5774. [7257]

1936 Morris 14 Series 11 sunshade saloon, original black cellulose, red leather, in genuine condition; £225.—A. BEEY AUTOS, rear of 44-46, Chase Side, Southgate, N.14 (near Tube). Palmers Green 4540. [7691]

Morris Twelve Cars Wanted
ROWLAND SMITH'S, the Morris 12 buyers.—Hampstead High St. (Hampstead Tube). Ham. 6041. [0949]

RAYMOND WAY, the hire purchase specialists are still buying pre-war Morris 12s and have unlimited cash available.—Canterbury Rd., Kilburn, N.W.6. Maids Vale 6044 (10 lines). [5043]

MORRIS FOURTEEN
Morris 14 saloon, £225; or 30 monthly instalments of £17/2/10.—Cooden Eng. Co., Bexhill, Tel. Cooden 600. [7276]

1937 model Morris saloon, completely overhauled, excellent condition; offer over £300.—721, Wel. 2024 before 7 p.m. Box 6329. [7131]

1937 Morris 14 streamlined de luxe saloon, special offer.—Bray Motors, 180-184, West End Lane, N.W.6. Hampstead 5490. [8040]

MORRIS TWENTY
95 gns.—Morris 20, 1935 model, de luxe 4-door saloon, dark blue, sliding head, blue leather, very good condition, taxed; terms, exchanges; list; open 9-7 weekdays and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). Hampstead 6041. [7923]

MORRIS OXFORD
CAR MART, Ltd.
MORRIS OXFORD 1949 saloon, 5,000 miles; £1,065.—Car Mart, Ltd., 320, Euston Rd., N.W.1. Euston 1212. [7590]

MANN EGERTON & Co., Ltd., offer:—

1949 Morris Oxford saloon, black with beige leather upholstery, 9,000 miles, immaculate condition throughout; £995.—Berkeley St., London. W.1. Regent 2073. [7431]

1949 Morris Oxford saloon, grey, beige upholstery, 11,000 miles, as new; £1,195.—Berkeley St., London. W.1. Regent 2073. [7431]

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1949 Morris Oxford saloon, grey, beige upholstery, 11,000 miles, as new; £1,195.—Berkeley St., London. W.1. Regent 2073. [7431]

WE are cash buyers of all Morris post-war models.
—The Warren Motor Co., 353-355, Euston Rd., London, N.W.1, Euston 7751. (1688)

Morris Spares and Service
FOR Morris mudguards, running boards, 1930-46.—
Brooks, 85, Queen's Rd. E., Brighton. 10384
LARGEST and quickest spares service in the South of
England.—Hewens Garages, Ltd., Reading. 10206
4436.
DALTON MOTORS for Morris service and spares.
1934-49 models.—317 Kingsland Rd., Dalston, E.8.
Clissold 4943. (12315)

NASH
NASH Ambassador 8 1939 coupe with dickey, fine condition; £375.—263, Belgrave Rd., S.W.1. (4962)

OLDSMOBILE
JOE THOMPSON (MOTORS), Ltd., offers:—

1948 Oldsmobile Futuramic de luxe saloon, colour maroon, very low mileage.
JOE THOMPSON (MOTORS), Ltd., 97, Fulham Rd., South Kensington (next to Michelins). Ken. 4858.
DISTRIBUTORS (RAWLEY), Ltd., Sales, Service and Spares, Blandley Heath, Nr. Lingfield, Surrey. Tel. Lingfield 330-1.

1936 6-cyl. Oldsmobile saloon in really good order; £325.
DISTRIBUTORS (RAWLEY), Ltd., Blandley Heath, Nr. Lingfield, Surrey. (10113)

1937 Oldsmobile 27hp drop head fourseater, finished black, in superb order throughout; £350.—Pantiles Service Garage, London Rd., Guildford 5326. (7700)

1947 Oldsmobile 4-door saloon, first registered 1948, hydraulic drive, radio, heater, etc., low mileage.—Sidney Marcus, Ltd., 35, Sloane St., S.W.1. Tel. Sloane 3557-6970. (7687)

1939 6-cyl. Oldsmobile fixed head coupe, steel, 1948, column gear change, radio, heater, in beautiful condition throughout; £475.—"Pinehurst," Muddford, Cheshire. (10450)

OLDSMOBILE main dealers for London, Middlesex, Essex and adjoining counties.—Lex Garages, Ltd., Service, 2, Lexington St., W.1 (Gerrard 8600). Spares Parts: Kensington Place, Camden Hill Rd., W.8 (Park 8611). (10576)

OPEL
265 gns.—Opel Olympia (reg. April, 1941), 1½-litre saloon, maroon, brown leather, very good condition; terms, exchanges.—Rowland Smith, below:—

195 gns.—Opel Cadet (Dec. 1937), 12hp saloon, maroon, good condition, taxed; terms, exchanges; list; open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube), Hampstead 6041. (7953)

OPEL Cadet cabriolet, in perfect condition, just overhauled, new upholstery, new battery, hood recovered, 40 mpg, owner going abroad; £280.—Cholmondeley Tapper, Bishopstone, near Aylesbury. (7630)

Cars Wanted
ROWLAND SMITH'S, the Opel buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041. 10980
PRIDE & CLARKE, Ltd., the Opel distributors, offer immediate cash payment for all models.—297, Tottenham Hill, S.W.2. Tel. 3664. (6099)

Opel Spares and Service
REPAIRS, new crown wheels and pinions, spares; reconditioned engines, suspensions, etc.—Tarrant & Fraser, 10, Windsor Rd., W.3. Pri. 2647. 10243

MAYNOR MOTORS, Ltd., distributors; Opel spare parts and reconditioned engine service.—Southampton St., Southampton, Hampshire. Tel. 3266. 10243

PRIDE & CLARKE, Ltd., new brake and clutch linings, valves, springs, piston rings, Opel Cadet crown wheels and pinions now in stock; quotations.—Stockwell Rd., S.W.8. Pri. 6251.

PACKARD
DICKS CAR SALES offer:—

1938 Packard 33hp coupe, fitted radio, positively as new; £495.
DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maida Vale 6888-9. (7376)

JOE THOMPSON (MOTORS), Ltd., offers:—

1937 Packard limousine, colour black, face-forward occasional; £475.
1939 Packard 120 Clipper Super 8 de luxe, very low mileage, special English body, an outstanding car.

JOE THOMPSON (MOTORS), Ltd., 97, Fulham Rd., South Kensington (next to Michelins). Ken. 4858.
LEONARD WILLIAMS & Co. (1940), Ltd., Great West Rd., Brentford, Middlesex. Ealing 340. (7623)

PACKARD Sole Concessionaires, amongst other approved late model Packard cars, offer:—

1947 21st Series Clipper Six 4-door saloon, 29.4hp, 120, right hand drive, with radio and heater, 19,000 miles; £10 tax.
LEONARD WILLIAMS & Co. (1940), Ltd., Great West Rd., Brentford, Middlesex. Ealing 340. (7623)

1947 Packard 34hp Clipper d/l, excel. cond., private sale.—Jackson, 9, Weymouth St., W.1. (1680)

1937 Packard 120 Club saloon, exceptional condition, 32,000 miles; £425; exchange considered.

ERIC HAYES, Ltd., 22, Conduit Mews, Hyde Park, W.2. Tel. Paddington 0289. (7975)

1939 Packard 27hp drop head coupe, last of four years, perfect, privately owned.—Aonley Leigh-on-Sea 78206. (7507)

1947 Packard Clipper, 4-dr. saloon, 12,000 miles, overdrive, radio, heater, etc.—Syd Abrams, Ltd. Bro. 2284-5. (15591)

1939 (May) Packard 6-cyl. touring saloon, black, rivet chain interior, genuine mileage 25,000, one owner, taxed end of year, in splendid condition; £750.
LEE CARS, Ltd., 60, Queenstown Rd., S.W.8. Tel. Galsworthy 3363. (7645)

395 gns.—Packard 8 (May, 1938), 35.8hp 4-seater 1935 convertible coupe, black, brown leather, terms, exchanges; list; open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube), Hampstead 6041. (7954)

£500.—Packard saloon, in very nice condition, finished gunmetal, with cream Bedford cord upholstery, first regd. 1949, £10 tax; sell, or will exchange for Super 8 limousine, must have division and occasional;—Ham 1240. (1680)

1946 (Oct. 1939) Packard 120 saloon, steering column gear change, two fastidious owners, positively immaculate condition, maintained regardless of expense, radio, main extras; £450; terms, exchanges.—Carmasters, 35-7, Upper Elmers End Road, Beckenham, Beckenham 6978. (7072)

1937 Packard Super 8 black saloon for sale; 55,559 miles only, bodywork and interior in absolutely immaculate and is as good as a modern model, the performance is superb and is all that may be expected from precision-built engine of this mileage; £725.—Bushell, Camberley 1498. (16535)

1937 Packard 8, black, one owner, taxed, recently overhauled, excellent car; £375.—Cunningham 1191.

W. J. REYNOLDS (MOTORS), Ltd., Main Ford & Fordson Distributors have pleasure offering from our huge stock of used cars of all type, a Packard drop head fourseater 1938, fitted with reconditioned 6-cylinder engine, good tyres, etc.; £550.—Ford House, New Rd., Dagenham. Tel. Rainham 770 (8 lines).

LEMOINES 1939 Super Eight partition, widest occasional, black, private, mileage 42,000, another similar genuine 26,000 (unused 10 years) £1,005.

A LPE & SAUNDERS (100 Limousines: Lists posted). Providence Court, Grosvenor Square. 2941-Mayfair.

Packard Cars Wanted
ROWLAND SMITH'S, the Packard buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041.

SIMPSON'S MOTORS (WEMBLEY), Ltd., wish to purchase all models Packard, Wembley 5903. (8003)

JOE THOMPSON (MOTORS), Ltd., Packard specialists, 97, Fulham Rd. (next door to Michelins), S. Kensington, Kensington 4858. (1934)

1935-6 Packard super 8 limousine, must be in excellent condition, require good mileage. Packard cars.—Great West Rd., Brentford, Middlesex. Ealing 3400. (10191)

Packard Spares and Service
LEONARD WILLIAMS & Co. (1940), Ltd., sole Packard concessionaires, Great West Rd., Brentford, Middlesex. Tel. Ealing 3400. (10469)

JOE THOMPSON (MOTORS), Ltd., Packard specialists, 97, Fulham Rd. (next door to Michelins), S. Kensington, Kensington 4858. (4351A)

PONTIAC
DICKS CAR SALES offer:—

1947 registered 48 Pontiac drop head coupe, numerous extras, fitted electric hood, radio, air conditioning, terrific performance, 12,000 miles only; £1,550.

DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maida Vale 6888-9. (7376)

PHILIP RICKARDS, Ltd., offer:—

1948 Pontiac sedan, left-hand drive, blue, perfect.—4, Brick St., Park Lane, W.1. Grosvenor 4772-3. (7294)

JOE THOMPSON (MOTORS), Ltd., offers:—

1949 Pontiac saloon, small mileage, right-hand drive.
JOE THOMPSON (MOTORS), Ltd., 97, Fulham Rd., South Kensington (next to Michelins). Ken. 4858.

Pontiac Cars Wanted
SIMPSON'S MOTORS (WEMBLEY), Ltd., wish to purchase all models Packard, Wembley 5903. (8003)

RACING CARS
CONTINENTAL CARS offer:—

MASERATI 2.9 Monoposto, ex-Bira, raced this season; £2,000.
MASERATI 2.9 Monoposto, excellent condition; £1,250.

MASERATI 2.9, ready for 2-seater body; £600.
MANY spares available.

3.3-litre a/c Bugatti, ex-Brian Lewis, Works Team, Grand Prix car, road equipped, Bugatti blue, absolutely faultless in every detail.
PORTSMOUTH Rd., Send, Surrey. Tel. Ripley 3122-3. (8055)

ERIC BRANDON'S aluminium Cooper, holder many records, taking 500 or 1,100 engines, many extras, ready to race; £375.—Folkestone, Hill Court, Surbiton. (6924)

500—1,000 c.c. Cooper racing car, new May, 1950, with or without 500 4-stud J.A.P. engine, first-class condition throughout; available immediately.
Box 6456. (7008)

COOPERS GARAGE (SURBITON), Ltd., of Surbiton, Tel. Elm. 350. (7623)

Great Britain of the Cooper 500 and 1,100cc formula racing cars. (8621)

1938 6C Maserati, together with full racing equipment and numerous spares, successful 1950 season; bargain for very quick sale, or will exchange post-war saloon.—Box 6299. (7058)

1950 1,000 J.A.P. and spares, including special alloy barrels, long-range tank, etc.—Miss Hays, Nutbourne Common, Farnborough, Sussex. (6332)

1100—J.A.P. engine, complete ready to race Castle Coombe, etc., possibility purchaser drive Goodwood; £625.—Michael Christie, Haddenham, Bucks. (7514)

COOPERS 1949, perfect condition, fitted with 500 c.c. J.A.P. engine, Norton gear box, long-range fuel tank, etc., suitable to take 1,100 c.c. engine, complete with special Cooper trailer; £250; can be seen by appointment.—Ty. 96, London Rd., Sevenoaks, Kent. (16816)

GEORGE WICKEN offers his 500cc Cooper-Jap, which proved so successful at the Brands Hatch International meeting on Aug. 13, this vehicle has had a number of modifications carried out and fitted with a number of extras; best offer over £475.—Tel. Barming 86162. 372, Tonbridge Rd. (6246)

Racing Cars Wanted
COOPER chassis required, or complete car less engine.

H. R. MARTINDALE, Ltd., 14, Harrison St., Leeds, 1. (7521)

RAILTON
1936 Ralston saloon, good tyres, well maintained throughout; £320.—Stone, Staffordshire 415.

MAJOR J. P. S. BARBER, 65, Linden Gardens, W.2. Baywater 6753.—All models up to 1947, 17hp, 27hp, 28hp coupes, saloons, touring cars. (7514)

OCTOBER 1936 28hp drop head coupe Ralston, first-class condition throughout, exchange for 20hp sports tourer or £400 cash; no offers; no dealers.—Bradford, Minver Cottages, Harrogate. (16243)

28 28hp Ralston drop head coupe, first registered 1936, this car is literally like new in every way; the interior is better than many six months old cars, checked over and passed as perfect by Hudson Motors; £600.—Welbeck 3991. (6672)

Ralston Cars Wanted
CASH immediately for good Ralston.—H. F. Edwards, 28, Upper High St., Epsom 9400. (7771)

RENAULT
RENAULT cars, spare parts, repairs & service.—Renault, Ltd., Western Ave., Acton, W.3. Acton 4556

£295.—1937 Renault 18 streamlined saloon de luxe, numerous spares, £225.—Farnworth, 28, Crown Lane, Wer. End Lane, N.W.6. Hampstead 6520. (8037)

CAR MART, Ltd.

RENAULT 8hp 1949 saloon, 3,000 miles; £645.—Car Mart, Ltd., 150, Park Lane, W.1. Grosvenor 300.

WELHAM'S RENAULT SALES & SERVICE, Surbiton Hill Rd., Surbiton, Elmbridge 1873, offers:—

RENAULT 1939 26hp 6-str. saloon, black, reconditioned; £395.
12hp saloon de luxe, blue; £325.

1938 12hp saloon de luxe, green; £310.

MAYFAIR GARAGES, Ltd.—1939 8hp de luxe saloon, lawn, good tyres, smart car, in well maintained condition throughout, 3 months' guarantee; £275.

MAYFAIR GARAGES, Ltd., Baiderton St. (opp. N. Bridge's clock), Mayfair, W.1. Mayfair 1004. Open 9-6, Sats. 9-1. (7600)

£595.—Right-hand drive Renault 26hp 7-seater 1946, finished black with fully chromed horizontal barred radiator and other very modern features, previously privately owned, with a much smaller mileage than average for this type of car, outstanding bargain, order, fully guaranteed by:—

CAMDEN MOTORS, Lake St., Leighton Buzzard, Beds. Tel. 401-2-3. Write for our 18-page, post free catalogue of nearly 400 fully guaranteed used cars; new and confidential hire purchase facilities; part exchange, free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive. (7605)

Renault Cars Wanted
ROWLAND SMITH'S, the Renault buyers.—Hampstead High St. (Hampstead Tube), Ham. 6041.

WELHAM RENAULT SALES & SERVICE, Surbiton Hill Rd., Surbiton (Elmbridge 1873), purchase all models. (1037)

RILEY
BROWN'S for Riley.

1946 Riley 1½-litre saloon de luxe, immaculate condition.—Brown's Garage, Loughborough (Essex) 4119 Tube. (7530)

TOM GARNER Ltd., offer

1949 Riley 2½-litre saloon, black with red leather and cloth, 7,000 miles.
1949 Riley 1½-litre saloon, black with green leather, 7,000 miles.

TOM GARNER Ltd., 10-12, Peter St., Manchester 1. Blackfriars 9265-6. (7390)

DICKS CAR SALES offer:—

1940 Riley 12 saloon, really well kept, definitely unmarked; £525.—Below.

1938 Riley 16 saloon fitted overdrive, amazing performance; £525.—Below.

1936 Riley Adelphi 12hp saloon, genuine bargain; £285.
DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maida Vale 6888-9. (7376)

G. F. BALHAM, Ltd., offer:—

285 guineas.—1936 Riley 12hp 1½-litre sports saloon, excellent condition, any trial.—Balham Hill, S.W.12 (100yds Clapham South Tube) Balham 3117. (7390)

GUY SALMON AUTOMOBILES, Ltd., offer:—

1947 Riley 2½-litre saloon; £1,005.—Portsmouth Rd., Thames Ditton, Emsworth 5551-2-1.

1936 Riley Adelphi saloon, well maintained by enthusiasts.
HILLINGDON MOTORS, 325-7, Long Lane, Western Ave., Hillingdon, Tel. Uxbridge 412. (720)

RILEY 1½-litre saloon, 1935, 16,000 miles, leather, 1935, 15,000 miles, 1935, 15,000 miles. (720)

OSBERRY GARAGE, Ltd., 165, Tottenham Lane, R. London, N.8. Mou. 2261 and 3019. (710)

FOR pre-war Rileys consult Browne & Sons, Motor Engineers, Loddon, Norfolk. Tel. 515-118. (140)

SUSSEX specialists for reconditioned Riley cars, repairs, spares.—Lewes Motors, Ltd., Lewes. (710)

1932 Riley 9 Gamecock, 700 overhaul, black, shiny, 2-seater; £130.—31, Fernham Rd., West. (750)

1949 Riley 1½-litre, maroon, beautiful condition, 8,000 miles; £1,250.—Roffey, Tel. Burch Heath 110. (750)

PERFORMANCE CARS. of Dealeham Mews, Beils Lane, N.W.3 (Hampstead 1111), offer with 3 months written guarantee:—

1932 rebuilt 4-cyl. 4-seater; £125; 1931 14/6 4-seater; £95; nothing else at time of going to Press. (601)

95 gns.—Riley 9 1929 4-door saloon, black, maroon wheels, leather upholstery; terms, exchanges.—Rowland Smith, below:—

395 gns.—Riley 1937 model, 1½-litre Kestrel 4-door saloon, black, sliding head, maroon leather, p-selector, excellent condition; terms, exchanges; list; open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube), Hampstead 6041. (7954)

1949 Riley 2½-litre saloon, 9,000 miles.—Brittas Lane, W.C.2. Tem. 3598. (740)

1949 Riley 1½-litre saloon, 7,000 miles only, very clean, with overdrive.—Ripco, Ltd., 16, Abchurch Lane, W.1. Regent 2952. (740)

NORMAN ROUNDHILL, Ashstead, offers 1947 1½-litre saloon, low mileage, black, red leather, immaculate, taxed December; £295.—Tel. Ashstead 34. (640)

GORDON CARS (LONDON), Ltd.—1949 Riley 2½-litre saloon, black, 9,000 miles, immaculate.—Gordon House, 373, Euston Rd., N.W.1. Euston 6611. (740)

1947 2½-litre saloon, black/red upholstery, excellent condition; £1,125.—H. A. Saunders, 40, Golden Green, E. N.W.11. Speedwell 6011.

RILEY 9 1933 Trinity special sports d.h. coupe, new R hood, sound condition, taxed end of year; £197, or nearest offer.—Tel Abbey 3093, after 5.30 Macaulay 3998. (6310)

1938 Riley 16hp Big Four, first registered March, close-coupled saloon, colour black, taxed 1 Sept.; £545.—Kitchener 28, Bushmead Ave., Bedford 5108. (1610)

1938 Riley 9 Monaco sports saloon, special £650, twin carbs, genuine mileage 25,000, original car in immaculate condition; £395.—Tel. Orpington 4765. (879)

1936 Riley Adelphi saloon, black/brown leather, excellent condition, colour black, taxed 1 Feb. Limited, Mid-Surrey Car Centre, Guildford Road, Woking, Tel. 1282. (7770)

1934 9hp Kestrel, had £100 overhaul, at present doing over 40 on single downbrought Zenith numerous spares; £225.—Farnworth, 28, Crown Lane, Horwich, Bolton. (620)

1932 Riley 9 Monaco saloon, immaculate condition and perfect runner; £185.—Lewis H. Barnhall, Portchester Square Mews, Bayswater, W.2. 2554.

1948 (June) Riley 1½-litre saloon, black, all brown leather upholstery, mileage 13,000; £1,200.—Canning Day, Ltd., Newport, Isle of Wight. Tel. Newport 2212.

1948 Riley 2½-litre saloon, speedometer reading 14,000, black, condition as new, trade enquiries welcomed.—H. C. Paul, Ltd., 32, Bruton Place, W.1. Mayfair 9201-2.

RILEY Sprite 1½-litre sports 2-str., June, 1937, in original condition, 21,000 miles only and like new, latest modifications; what offers?—D. J. Whitlick, Ashton Keynes, Swindon. (7509)

ROSE & YOUNG, Ltd., offer 1937 Riley 1½-litre Kestrel saloon, black, green leather; £395.—65-69, St. James's Place, W.1. (1 minute) St. James's Hill Station). Tulsa Hill 6464.

1936 Riley 1½-litre Merlin saloon, really in first-class mechanical condition, can be smart appearance, taxed Dec., good tyres, new battery, engine owned and well maintained; £300.—Keele, 1, Lamballe Place, N.W.3. (2551)

£265 Special series sportsman's 4-light saloon, finished black, very smart streamlined and modern appearance, remarkable performance for a 9hp car, enthusiast should not miss this one.

CAMDEN MOTORS, Lake St., Leighton Buzzard, Beds. Tel. 2041-2-3. Write for our 18-page, post free catalogue of nearly 400 fully guaranteed used cars; easy cash, confidential hire purchase facilities; part exchanges; free delivery; showrooms open till 8 p.m. Mon.-Sat. inclusive.

RILEY 2½, 1947, 25,000 m., black with red upholstery, fitted H.M.V. radio, immaculate condition, recently overhauled and renovated throughout; genuine offer, open any test; £1,600; no dealers; see and inspect any day by appointment.—Tel. Worthing 3440, Ref. Wel.

1948 (Jan.) Riley 1½-litre saloon, black, all brown leather, 13,000 m., as new. £1,075. (1947) Riley 1½-litre saloon, black, all red leather, 17,000 m., radio, £950. (1946) Riley 1½-litre saloon, black, all green leather, 21,000 m., perfect. (1947) Riley 2½-litre saloon, black, all brown leather, 23,000 m., £1,100.—Gibsons Sports Cars (Christchurch), Ltd., Lyndhurst Rd., Christchurch, Hants. Tel. 1681.

Riley Cars Wanted

C THE CAR MART, Ltd., wish to purchase Riley cars.—150, Park Lane, W.1. Grosvenor 3434. (0969)

ROWLAND SMITH'S, the Riley buyers.—Hamstead Rd. St. (Hamstead Tube). Ham. 6041. (0933)

D THE Riley buyers.
DICKS CAR SALES, 335-401, High Rd. Kilburn, N.W.5. Maids Vale 6888-9. (5730)

CYRIL SHEPARD requires:—
POST-WAR Rileys.—102, King's Rd., Reading 2712.

CASH buyers of low-mileage 1½-litre Rileys, direct or no object.—Blattons, Lord St., Southampton, Tel. 2268.
RILEY 9 sports tourer, 1935 or later, wanted urgently, within 50 miles of London.—56, The Burroughs, K.1. Tel. 1681.

REQUIRED privately, 1938-9 Kestrel 16/4, fitted manual gearbox; condition must be first-class. Box 6952.

Box 6952 Riley distributors, will purchase any non-Bovarian Riley cars.—110, Bold St., Liverpool. 1, Tel. Royal 6622.

JACK ROSE, Ltd., require low-mileage Riley cars.—A. Tankard & Smith, Ltd., 194/196, Kings Rd., Wallington 6677-8.

APPROACH us first before disposing of your Riley car.—A. Tankard & Smith, Ltd., 194/196, Kings Rd., S.W.3. Faxman 4801/2-3.

URGENTLY required, 1946-9 1½-2½-litre saloons.—Rex Rente, Riley Specialist, Sharnbrook Lane, Betches, Southampton. Tel. Botley 132.

MOTORISTS (LONDON), Ltd., are immediate cash buyers of post-war Riley saloons.—Great North Rd., Finchley Station N.2. Tel. 2301-2.

URGENTLY required, low mileage 1947-9 Riley 1½- or 2½-litre saloons.—Gibsons Sports Cars (Christchurch), Ltd., Lyndhurst Rd., Christchurch, Hants. 1681.

Riley Spares and Service

ACOT MOTORS, Ltd.—Preselector gear boxes: exchange and repairs.—169, Fulham Rd., S.W.3. Kensington 7501.

DOON & PORTER, Ltd.,
RILEY distributors.—Spares and specialised service in Castelnau, Barnes, S.W.13. Riverside 4444. (0147)

RILEY service consultant the Riley specialist.—W. T. Mason & Co., 2, Ley St., Ilford. Tel. Ilford 4071.

URGENT and quickest spares service in the South of England.—Hewens Garages, Ltd., Reading. Tel. 4456.

HARTLEY'S for Rileys, spares and service.—185-187, H Stanstead Rd., Forest Hill, S.E.23. Forest Hill 2244-5.

WARD & CO. (PUTNEY), Ltd., coach and mechanical repairs, quick and efficient service.—36, Putney Rd., Putney, S.W.15. Put. 5751-2-3. (0892)

WHEN in the West consult the Riley specialists: comprehensive stock of spares and immediate attention.—Tassey Motors, Ltd., 176, Kellaway Ave., Bristol 7. Tel. 43069.

ROLLS-ROYCE

C THE CAR MART, Ltd.
ROLLS-ROYCE 1939 Wraith 7-seater limousine by H. J. Mulliner, 35,000 miles, £2,350.—Car Mart, Ltd., 150, Park Lane, W.1. Grosvenor 3434. (7502)

P

B LTD.
ROLLS-ROYCE specialists 40 years.

1934 25hp Rolls-Royce Park Ward saloon.

1933 40-50hp Rolls-Royce Barker sports saloon.

1932 25hp Rolls-Royce Hooper sports saloon.

1931 40-50hp super sports open 4-seater.

1931 40-50hp Rolls-Royce, originally fitted with full length 4-door, 4-light body by Windover, front part original, rear section has been modified for carrying goods, colour dark green and black and brown, leather upholstery; has been on C licence.

PADDON BROS., Ltd., 60, Cheval Place, South Kensington, London, S.W.7. Ken. 9477/8. (7607)

H HAROLD PERRY, Ltd., Invicta Works, 279, Ballards Lane, North Finchley, N.12. Tel. Hillside 4444.

1937 Rolls-Royce Phantom III passenger saloon, colour maroon, white line, reculいた completely, 6585 mechanical overhaul November 1949, the car is in immaculate condition; £2,000.

W HAROLD PERRY, Ltd., Invicta Works, 279, Ballards Lane, North Finchley, N.12. Tel. Hillside 4444.

FOX OFFICIALLY appointed retailers of Rolls-Royce and Bentley cars; 1936 touring limousine with folding hood on the Rolls-Royce 20/50hp chassis; excellent condition, ideal hire car.—H. A. Fox & Co. Ltd., 3-5, Burlington Gardens, Old Bond St., W.1. Tel. No. Regent 7687.

FOX RIPPON.
RIPPON.
RIPPON BROS., Ltd.

N NORTHERN Rolls-Royce specialists since 1905.
1939 Wraith close-coupled limousine with Rippon body.

1937 Phantom III 7-seater limousine by Rippon, black with fawn West of England cloth, very small mileage.

1936 touring limousine by Rippon.

1935 Phantom II drop head coupé by Gurney Nutting, excellent condition.

1934 25hp 4-door sports saloon by Rippon.

1930 25hp 7-seater limousine by Rippon.

YOU can buy with confidence from the largest distributors.

RIPPON BROS., Ltd., Huddersfield 6340 (5 lines).
B BROADLANDS, Bradford, Leeds and Sheffield. (0905)

1936 25-30 owner-driver saloon, body by Barker, grey and blue, recently overhauled, immaculate throughout.

103 New Bond St., W.1. Tel. Mayfair 8351-6. (7536)

PADDON BROS., Ltd.
ROLLS-ROYCE specialists 40 years.

P PICKED selection small Rolls-Royce saloons and limousines, in perfect condition; please ask for list and booklet.

60, Cheval Place, London, S.W.7. Kensington 9477. (0097)

DICKS CAR SALES offer:—
1939 Rolls-Royce 30hp Wraith saloon, fitted H.M.V. radio, very fine order; £2,250.

DICKS CAR SALES, Ltd., 399-401, High Rd., Kilburn, Maids Vale 6888-9. (7377)

JACK OLDING, of Mayfair,
OFFICIAL Rolls-Royce and Bentley retailers,
OFFER:—

PHANTOM III Rolls-Royce sports saloon by Park Ward, black with blue leather, first registered August, 1937.

25-30 Rolls-Royce saloon with division by H. J. Mulliner, "E" column, finished black with beige leather to rear and black to front, immediate delivery, first registered May, 1937.

20-25 Rolls-Royce with division by Park Ward, black with blue leather, first registered April, 1933; £995.

H

R H. R. OWEN, Ltd.,
17, Berkeley St.,

G GREAT Britain's leading specialists in Rolls-Royce and Bentley cars.
P PROUD members of the Swain Group.

A NATIONAL Motoring Organisation.
1934 Rolls-Royce 20/25 limousine by Thrupp & Maberly, colour black with navy blue leather to front and beige cloth to rear upholstery. Ref. H.5152.

1935 Rolls-Royce 20/25 4-door 4-light saloon (SR) by Freestone & Webb, colour black with grey hide upholstery; this car is fitted with twin side mounted spares and cover, radio. Ref. H.5080.

1935 Rolls-Royce Phantom II sports saloon with division by Hooper, colour black with blue leather upholstery, fitted with two side mounted spares. Ref. H.4478.

1936 Rolls-Royce Phantom III 4-door 6-light S.H. saloon by Hooper, colour black with brown leather upholstery. Ref. H.5013.

1938 Rolls-Royce Sedanca de Ville 25/30 by H. J. Mulliner, in black with fawn hide upholstery; this car is fitted with picnic tables and a side spare. Ref. H.2072.

1939 Continental Close-coupled limousine by Park Ward, in maroon and black with black leather upholstery; a really beautiful car. Ref. H.5012.

1947 (SR) Rolls-Royce Silver Wraith touring limousine, colour black with brown hide to front and beige cloth to rear. Ref. H.5087.

1948 Rolls-Royce Silver Wraith touring limousine, colour black with brown hide with brown hide upholstery, picnic tables, cocktail cabinet, wing valances, overriders front and rear. Ref. H.4654.

ALL cars carry our unique 6 months' guarantee. Please write or phone for details to:—
H. R. OWEN, Ltd.,
17, Berkeley St., London, W.1.
TEL Mayfair 9060 (10 lines). (7300)

H HAROLD RADFORD & Co., Ltd.
INVITE you to call and inspect their unique selection of Rolls-Royce cars.

1939 Rolls Wraith four-light, razor edge saloon with disappearing division and built-in boot by Windover, black with black leather in front and cloth at rear, mileage 33,500, in beautiful condition throughout.

1939 (Nov.) Rolls Wraith sports saloon with two-toned grey radio and heater, grey upholstery with loose covers 50,000 miles, chassis No. WHC.50, in beautiful condition throughout.

HAROLD RADFORD & Co., Ltd., Melton Court, South Kensington S.W.7. Tel. Kensington 5642 (5 lines).
1938 Rolls-Royce 20/25 4-door 4-light saloon; £1,850. Also
1934 Rolls-Royce 20/25 5-seater 6-light limousine by Rippon; £775. Also
1937 Rolls-Royce Phantom III sports saloon, built-in boot; £1,750.

GEORGE NEWMAN & Co., 369, Euston Rd., N.W.1. (7412)
1925 Rolls-Royce 20hp cabriolet, sound condition, offers near £350.—Frobisher 0398. (7819)

ROLLS 20-25 4-door sports saloon by Hooper, registered 1933 (chassis 6611), 46,000 miles; £1,350 or offer.—Hen. 8675. (7112)

1928 20/25 Rolls-Royce 7-seater limousine, in good condition throughout, reasonable cost.—Northwood 3355. (6047)

ROLLS-ROYCE 26hp Windover very good condition; £1,150.—Brinds Garage, 14, Green Lane, Thornton, Tel. 3441. (1617)

ROLLS-ROYCE 1930 20hp saloon required; must be good condition.—Bayliss, 27, Metcalley Park Rd., Edgemark, Birmingham.

ROLLS 20hp 4-door sports saloon by H. J. Mulliner, leather throughout, face forward seats; £695.

1937 Rolls-Royce 7-passenger limousine, swept tail; £1,695.—94 Gt. Portland St., W.1. Lan. 1343. (7289)

PHANTOM III 1936 sedanca 31,000 miles, one owner, privately owned, R.R. service.—Alexandra Garage, Old Harrack Yard, S.W.1. Slo. 4121. (6825)

PRIVATELY owned Phantom II, Rolls Barker saloon, owner driver body, low built; £700, in perfect condition.—Steel, Brockhurst 2268. (6908A)

1935 Rolls Sedanca, 25hp, really smart; offers; condition—Steele, Brockhurst 2268. (7181)

CLARKE'S OF FIBRIGHT, officially appointed retailers of Rolls-Royce cars, offer 1933 20/25 Barker sports saloon in black with grey leather.

1935 Gurney Nutting sports saloon, very modern with grey leather, fitted with rear boot, finished in black with grey leather.

CLARKE'S OF FIBRIGHT, Fibrigh, Surrey. Tel. Brookwood 2201. (6564)

EDWARDS & CO. (BOURNEMOUTH), Ltd., Bournemouth. Tel. 1272-3 officially appointed Rolls-Royce retailers and repairers; reliable used cars in stock. (4971)

1929 40/50 Phantom I Rolls-Royce, Thrupp & Maberly 2-door close-coupled body fitted with Perkins 4 diesel engine, 32 mpg; offers?—Box 6130.

1937 Rolls-Royce 7-passenger limousine Hooper body, small mileage.—Evans & O'Malley, Loundes St., Knightsbridge, S.W.1. (Tel. Slo. 1535).

1939 Phantom III, 16,000 miles, virtually new Hooper semi-razor edged owner-driver saloon, outstanding appearance, every modification.—Box 6204. (6955)

1931 Phantom II Thrupp sedanca, in really outstanding condition, modern lines, taxed year; £475.—Basingstoke Motor Co., Ltd., Basingstoke, Tel. 477. (7876)

H H. R. OWEN Phantom Rolls, swept tail, 6-bearing, H chromium radiator, upright rear door, 6-wheels, disc, winding bier, good order. Needs painting; £340.—Priest, 2 Providence Court, Grosvenor Square, W.1.

ROLLS-ROYCE Phantom II 40-50hp, Park Ward body 7-seater limousine, drop division, 2 up-2 down disc wheels, Rolls maintained, superb condition, 48,000 miles, newly re-calibrated black, inspection by appointment.—Write Johnson, 17, Helens Avenue, Golders Green, London, N.W.11. (6210)

J
B

JACK BARCLAY, Ltd.

LARGEST Official Retailers of Rolls-Royce and Bentley: List of used models on request to 12-13, St. George St., Hanover Sq., London, W.1. Tel. Mayfair 7444.

£300—Rolls-Royce 20hp chassis, 1930-31, excellent condition throughout, regularly maintained and serviced.—909, Northolt Rd., S. Harrow, Byson 3666.

£1,000—Phantom II, Gurney Nutting sports saloon, excellent order, well maintained; £550.—Frank Dale, 66, Princes Gate Mews, Exhibition Rd., S.W.7, Kensington 6950.

£1,200—20/25hp and 25/30hp Hooper, Limousines, mileages all under 40,000, not ex-hire, from £1,200.—Lawton-Goodman, 36, North Audley St., W.1. Mayfair 3360.

1937—Rolls-Royce 30hp special body limousine by Barker, this car has done 29,000 and is in 1940 condition; £2,500 or near; year's tax.—James Fearn, Kirtlemair, Scotland. Tel. 59.

25/30hp—Rolls-Royce owner-driver 4-seater saloon, new body 1933, immaculate car; £1,150; terms, exchanges.—Moreton Garages, 51, Albert Embankment, S.E.11. Reliance 4016.

1938—Rolls-Royce 25/30 Mulliner, owner driver saloon, 48,000 miles only, superbly beautiful unmarked car; exchanges, terms.—Swannome Garages, 1176, Christchurch Rd., Bournemouth. Tel. Southbourne 1022.

950ccs.—Rolls-Royce 1935 20/25hp Thrupp and Maberly 7-passenger double-enclosed limousine, black, swept tail, upholstered front leather, rear cloth, sliding partition, face forward occasional seats, Ace discs, carefully used, excellent condition; terms, exchanges; list open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube), Hampstead 6041.

A & S (Limousine Specialists) offer unique section of Rolls-Royce (swept tail) Limousines, 1935-1938 Long Phantom (FY Series) 6-wheels, disc wheels, cab etc. exceptional, immediate delivery. £365.

1935—Rolls-Royce 1934 Double Enclosed 25hp. Windowed, widest occasional, black, exceptional, private.

1935—Rolls-Royce 1935 (ride control) 25hp. Hooper (swept tail) widest occasional, black, £2,000 (unused 10 years).

1937—Rolls-Royce 1937 Barker (30hp), widest occasional, L partition, black, 35,000 (unused 10 years) swept tail, immaculate.

1938—Rolls-Royce 1938 Thrupp 30hp, partition, widest forward, black, genuine 15,000, meticulously maintained, reasonable cost.

1938—Rolls-Royce 1938 Hooper 30hp, partition, wide occasional, swept tail, black, genuine 24,000, immaculate, reasonable cost.

1939—Rolls-Royce 1939 Wraith 30hp Thrupp, widest occasional, partition, black, genuine 11,000, delightful condition.

1935—Rolls-Royce 1935 Phantom II Barker, black, widest occasional, partition, exceptional condition, £1,170.

1938—Rolls-Royce 1938 Barker Phantom III, swept tail, L widest occasional, partition, 32,000, immaculate, seen.

ALPE & SAUNDERS always purchase Rolls-Royce, 14 A Limousines displayed. Genl.—Providence Court, Grosvenor Square. 2941-Mayfair. (7595)

Rolls-Royce Cars Wanted

URGENT demand is urgent.

OWNERS who have Rolls-Royce cars for disposal are invited to communicate with the Swain Group of Companies London office, H. R. Owen, Ltd., 17, Berkeley St., W.1. Tel. Mayfair 9060. Head Office, Hoffmann's Garage, Ltd., Huddersfield Rd., Halifax, Yorks. Tel. Halifax 5944.

THE CAR MART, Ltd., wish to purchase Rolls-Royce cars.—320, Euston Rd., N.W.1. Euston 1212.

J. MARSHALL.

WANTED, Rolls-Royce 20/22 and 20/25, all types of coachwork, any condition.

J. MARSHALL, 689, St. Albans Rd., Watford. Tel. Garston 5265. (5759)

ROWLAND SMITH'S, the Rolls-Royce buyers.—Hampstead High St. (Hampstead Tube), Hagn. 6041.

A & S always purchase modern 25/30hp Saloons with boot, also 7-passenger 25/30hp Limousine 1933/1948.

A & S urgently require Phantom II also Phantom III Saloons; also 7-str. Limousines. Alpe & Saunders, Providence Court, Grosvenor Square. Mayfair-2941.

SWANMORE CAR SALES urgently require Rolls-Royce cars.—Swannome Garages, 1176, Christchurch Rd., Bournemouth. Tel. Southbourne 1022.

BROADWAY MOTOR CO. require elderly Rolls-Royce cars, particularly 21.6hp and 25.3hp types.—1515, Russell Rd., Wimbledon. Liberty 2494. (6698)

WE are open to purchase any type pre-war Rolls-Royce cars, complete or otherwise.—Compton, 69, Westway St., Crystal Palace, S.E.18. Liv. 3362. (7497)

WANTED 1935-6 Phantom II Rolls-Royce sports saloon; full details and lowest price to—J. Hutchinson, 64 Mount Rd., Penn. Wolverhampton. Tel. 36643.

RIPPON BROS., Ltd., the Northern Rolls-Royce specialists, special retailers and repairers, wish to buy good late model Rolls-Royce.—Huddersfield 6340 (5 lines).

THE BASINGSTOKE MOTOR CO. purchase 20/22 F.W.B., 20/25, early 25/30 late Phantom II; any coachwork considered, tourists especially wanted.—Byson Rd., Basingstoke. (7879)

JACK OLDING, Ltd., 8-10, North Audley St., W.1, official Rolls-Royce and Bentley retailers are interested in the purchase of Rolls-Royce cars in first-class condition. Mayfair 5242. (6815)

IRELAND'S GARAGE, Ltd., Parkfield Rd., Wolverhampton (Tel. 36772), wish to purchase 1935-1938 Phantom II Rolls, any type body.—Full particulars and lowest cash price to above address. (7108)

CHARLES POLLETT, Ltd.—Official appointed repairers and repairers, buy good late cars—18, Berkeley St., W.1. May. 6266. Service works and stores: 12, Wellesley Ave., W.6. Ave. 1413. (8364)

Rolls-Royce Spares and Service
JACK BARCLAY, Ltd.

LARGEST official retailers and repairers of Rolls-Royce cars, servicing or complete overhauls, mechanical or coachwork. Large stocks of spares for all models.

WORKS—Lombard Rd., Morden Rd., Merton, S.W.12. Liberty 7224. (10625)

W. M. COOPER, Ltd., Catherine St., St. Albans 4346.

SPARES and service. The only officially appointed Rolls-Royce special retailers and repairers in the county of Hertfordshire.

CHARLES POLLETT, Ltd.—Officially appointed repairers and repairers, 18, Berkeley St., W.1. May. 6266.

SHOWROOMS: 18, Berkeley St., W.1. May. 6266.

SPARE PARTS.

SERVICE: 12, Wellesley Ave., W.6. Riv. 1413.

CENTRAL GARAGE, Croydon. specialists in servicing, complete overhauls, mechanical or coachwork, for all Rolls-Royce and Bentley models.—Central Garage, Tel. Croy. 7664.

H. A. P. Co., Ltd.—Officially appointed retailers and repairers, 3-5, Burlington Gardens, London, W.1. Tel. Regent 7687. Service works: 212, New Kings Rd., Fulham, London, S.W.6. Tel. Renora 3866. (10446)

14 spares for 20 and 25hp Phantom I and Phantom II chassis; also reconditioned replacement engines, guaranteed for 12 months, for the above models; full repair service at reasonable charges; also wheels, tyres, etc.—Compton, 69, Westway St., Crystal Palace, S.E.19. Liv. 3362.

MEBES & MEBES, Ltd. (Est. 1893), offer:—

1938—Rolls-Royce 10hp de luxe 4-door saloon, black, brown hide upholstery, recently reconditioned, tuned and in excellent mechanical condition, coachwork good, tyres as new, taxed Dec. 1939.—The Broadway, Mill Hill, N.W.7. Tel. Mil. 2000. (7712)

1947—Rolls-Royce 10hp 19,000, condition and appearance excellent; £965.—Box 6332, 7145

1936—Rolls-Royce 10hp, one owner, really amazing condition engine; £845.

1947—Rolls-Royce 10hp 4-door saloon, almost unused inside and out, speedometer reading 15,000; accept. £895.—Jack Rose, 11, Stafford Rd., Wallingford, Surrey. Wallingford 6677-8.

CEDARS GARAGE—All our used cars are carefully selected, tested and overhauled in our workshops before they are offered for sale with a 3 months' written guarantee and 2 free after sales services.

1939—Rolls-Royce 10hp black with blue leather, reconditioned engine; £895.—Lewisham, S.E.13. Tel. Lee Green 2920.

CEDARS GARAGE, Lee Terrace, Lewisham, S.E.13.

CAR MART, Ltd.

1947—Rolls-Royce 12 1947-8 touring car, 5,000 miles; £1,125.—1212, Car Mart, Ltd., 320, Euston Rd., N.W.1. Euston 1212.

1947—Rolls-Royce 11 saloon, low mileage.—Ripco, Ltd., 16, Albemarle St., W.1. Regent 2952. (7845)

1938—Rolls-Royce 12 sports saloon, exceptional condition, taxed Dec. 1939.—Northways Garage, 62, Woodside Park Rd., Finchley, N.12. (7065)

£525—1938 Rolls-Royce 12 sal, black/green hide up upholstery, engine recently reconditioned, good tyres, immaculate condition throughout.

MAKIN & HARRISON (MOTORS), 492-6, High Rd., Chiswick, Chiswick 0558/2619.

1939—Rolls-Royce 12.5 light saloon, colour black, one owner since new, taxed year, new tyres, all round, new battery recently fitted, genuine value; £550.

PARSONS & PARSONS (GARAGES), Ltd., Potter St., Harlow, Essex. (7257)

1947 (May) Rolls-Royce 12 saloon, 12,000 miles, black/brown, radio, many extras.—Tickford, Ltd., 8, Upper St., Martin's Lane, W.C.2. Temple Bar 3358.

£395—1937 Rolls-Royce 12.5 sports saloon, in exceptional condition throughout, bargain.

Bray Motors, 180-184, West End Lane, N.W.6. Hampstead 5450.

1947—Rolls-Royce 12 1948 January, sports Forliff saloon, 10,011, one owner, spare unused, just as new, reasonable offer, consider exchange, terms, A.A. examination guaranteed.—278, Chester Rd., Castle Bromwich. (6780)

£350—1936 Rolls-Royce 11 2-door 4-seater sports touring, good hood, screens, full tonnage and slip, recent respray, good tyres, engine, etc., excellent; seen London Monday to Friday, North Bucks week-ends.—Write J. Leigh, 54, Western Rd., Wolverton, Bucks.

1947—Rolls-Royce 14 sal. 1937, black, in first-class order, carefully maintained.—610, taxed Dec.—Newstead Hall, Horley, Surrey. Tel. Horley 451. (6179)

13500—Authenticated miles.—1947 (Nov.) Rolls-Royce 14 6-light saloon, indistinguishable new.—Lionel H. Pugh, 13-14, Brooks Mews, W.1. May. 4433. (7278)

1947—Rolls-Royce 14 saloon, black with brown leather throughout, one owner, heater, H.M.V. radio, low mileage, spotless condition, £1,050; exchanges.—S. H. Motors, 688, High Rd., Whitehorn, London N.20. Hillside 6671. (7527)

1938-9—4-light, sunshine roof, refinished in a modern leather interior, engine extensively overhauled including releveling, new pistons, new big-end bearings, etc., new valves and springs, this engine is equal to new and has not yet been run, the cost of this engine was over £100, a really outstanding car with a superior performance 10 to 80mph on top gear and a genuine 28mpg, brand new battery, tyres almost unmarked, the appearance of this almost as new car could easily be taken for a 1947 model, any examination. £255; terms and exchanges.—Maidstone Engineering Co., Cross St., Salford, 6. Tel. Pen. 3457. (7203)

R. F. FUGGLE, Ltd. offer:—

A genuine 9,000 miles 1947 16 Rover sports saloon, colour grey with blue leather upholstery, in almost new condition.

BUSHEY Heath, Herts. Tel. 1685.

1938—Rolls-Royce 16 black sunshine saloon, excellent order, recent, reconditioned throughout, £550.—Smitten, 17, Brougham Rd., Haverford, St. Aust. Trent.

1947—Rolls-Royce 16 saloon, black, red leather, sports interior, 12,000 miles, excellent condition, £1,200.—Garage, 1176, Christchurch Rd., Bournemouth. Tel. Southbourne 1022.

1947—Rolls-Royce 16, 4-light saloon, registered February 1943, black, beige upholstery, excellent condition, heater, small mileage, £1,100.—Lanforth, Brampton, West Clendon, Surrey. (6012)

1938—Rolls-Royce 16hp maroon saloon, good condition, steering, brakes and clutch retained, Ace discs, £1,200 or near offer; can be seen Victoria Station, S.W.1.

1938—Rolls-Royce 16hp 4-light sports saloon, modified, £1,250 or near offer; can be seen Victoria Station, S.W.1.

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NAYLOR & ROOF, Ltd.—1949 Triumph 2000 Roadster, smoke grey, leather upholstery (reg. June, '49), very carefully maintained, low mileage, any trial or inspection. £995; three months' guarantee; choice of 250 quality cars; demonstrations free within 100 miles; terms available—25, East Hill, Clapham Junction, S.W.16. Bus. 5272. Open 9.45 each week-day including Saturday. (7956)

Triumph Cars Wanted

C M THE CAR MART, Ltd., wish to purchase Triumph cars—320, Euston Rd., N.W.1. Euston 1212. **CYRIL SHEPHERD** requires—

POST-WAR Triumphs—102, King's Rd., Reading 2712. **ROWLAND SMITH'S**, the Triumph buyers—Hamstead High St. (Hamstead Tube). Ham. 6041.

M **ARSTON MOTOR CO., Ltd.**, for your Triumph—N.15. Tel. Sta. 8000. Seven Sisters Rd., Tottenham 10182.

CASH buyers of low-mileage 1800 and 2000 Triumphs; distance no object.—Hattens, Lord St., Southampton. Tel. 2268. (10804)

Triumph Spares and Service

S & T SERVICE and spares for all models.

MANUFACTURERS' largest stockists in Britain of Standard and Triumph spares and service exchange assemblies.

STANDARD & TRIUMPH SALES, Ltd., London Distributors, Junction Boundary Road and Abbey Road, St. John's Wood, N.W.8. Maida Vale 9114 (10 lines). **NEWNHAMS, Ltd.**

TRIUMPH specialists; service and spares for all models, including Dolomite type radiator grills.—Newham House, 235-7, Hammersmith Rd., W.6. Riv. 4646. **ASHL ROY, Ltd.**—Triumph spares, complete stock. B wholesale and retail.—161, Gt. Portland St., W.1. Langham 7733.

TRIUMPH spares for all post-war models; largest provincial stockists.—Hollingsrake Automobile Co., Ltd., Stockport (Tel. 4464); and Prince's Drive, Colwyn Bay (Tel. 3522). (10355)

UTILITY CARS

G. P. (SALHAM), Ltd., offer—

130 gns.—1936 Morris 6 4-door utility, replacement engine, genuine bargain.—25, Balham Hill, S.W.12. (100 yards Clapham South Tube). Battersea 5117. **HUDSON** utility 1936 23hp 4-door; £325 or offer.—Box R.3529, Bensons, Kingsway Hall, Kingsway. W.C.2. (6489)

£335—1936-40 Austin 3 Utility, very smart looking, very excellent runner, seating for a large space behind.—Below.

£445—1947 (reg.) Morris 10 Utility, excellent car, 1947 body, second engine, recently recoloured cream.—Below.

£595—1949 model Morris 12hp with excellent engine, wooden coachbuilt Utility body, 8,000 miles genuine, literally brand new, unrepeatable opportunity. **AMIES & CO.** 100 Finchley Showrooms, 421, High Road, N.12. Finchley 6221. (7806)

JOWETT Bradford utility, 1947, fitted special body, very attractive vehicle.—Autowork, Ltd., Winchester, W.1. Winchester 4384. (7490)

HUMBER Snipe ex W.D. utility, first registered Jan. 1948, very good condition, small mileage; £450.—47, York Rd., Weybridge, Surrey. Weybridge 3718.

LA-FRANCIS 1949 (May) shooting brake, as new condition, low mileage; £650.—Staveley, Well House, Colliingbourne, Kingston, Wilt. (6851)

1950 reg. Canadian Mercury V.8 station wagon, maple body; £10 tax; colour gear shift, everything 100%; £465; no dealers.—Box 6296. (7034)

1949 12hp utility, as new, 7,000 miles. £1050. bench type seat, sliding windows; £495.—Howard, 10, Orchard Ave., N.20. Hillside 2014. (6673)

BRADFORD Jowett de luxe, first registered January, 1949, excellent condition; 13,000 miles; £500.—Mrs. Crookenden, Hambro's Manor, Tonbridge, Kent. (1st registered) Hillman Minx utility, excellent condition throughout; 2375.—Bertram Cowen, Hermitage Lane, Streatham, Fulham 2100. (7194)

1949 Bradford utility, one owner, genuine 3,000 miles, immaculate, quite beautiful, Tel. 2268. exchanges.—B. & H. Motors, 1468, High Rd., Whetstone, London, N.20. Hillside 6671. (7528)

1937 Bulk utility (steel body), 10 seats folding into four, very good condition, price £320. or near offer.—Cheshington Salvage Co., Ltd., Church Lane, Cheshington, Surrey. Epsom 4026/7. (6259)

A TRACTIVE coach-built Hillman Minx utility, owned since 1948, registered November, 1948; mechanically perfect, recoloured, good tyres, new battery, exceptional condition. £425; or near offer.—Cobham (Surrey) 115. (6539)

HAROLD RADFORD & Co., Ltd., specialists in re-designing and building wooden metal-panelled shooting brakes and metal Panelmats dual-purpose van bodies, etc.; selection in stock for immediate delivery; tel. or call for full details.—Harold Radford & Co., Ltd., Melton Court, South Kensington, S.W.7. Ken. 6642.

GOOD utilities of post-war chassis required, 8-16hp. **HAROLD RADFORD & Co., Ltd.**, Melton Court, South Kensington, S.W.7. Tel. Kensington 6642 (5 lines).

BRAKE wanted, 12-14hp; view London area; details, price.—Box 6393. (7208)

CASH buyers of low-mileage utilities; distance no object.—Hattens, Lord St., Southampton. Tel. 2268.

ROWLAND SMITH'S, the Utility car buyers—Hamstead High St. (Hamstead Tube). Ham. 6041 (0955)

WANTED, shooting brake, write full particulars.—Hickman, 41, High St., Beckham, London, S.E.15. (0955)

POST-WAR shooting brake wanted, 25 to 30hp; must be in good condition.—Apply Eagle Construction Co., Ltd., East Common Lane, Scunthorpe. Tel. 3757.

Vauxhall 10, excellent condition throughout, recoloured engine; £610.—Box 6171. (7386)

£345 Vauxhall 10, 1938 saloon, original condition, very well kept, small mileage, choice 4; many others.—Benmores, 1, Clarendon Rd., Holland Park, W.11 (50 yards Holland Park Tube). Par. 3266/7. Mon.-Sat. 9-5. (7386)

1939 Vauxhall 10, excellent condition, new engine 7,000 miles, new battery, recoloured, new steering and rear axle overhauled June, '49; privately owned. Fr/Sa. 2365.—34, Ousey Crescent, St. Albans. Tel. 5470. (7035)

1947 Vauxhall 12, black, 18,000 miles, one owner; £335.—Shepperton Supplies Co., Ltd., Shepperton, Walton 2322. (7127)

1947 (April) Vauxhall 12 saloon, black, one careful owner, really exceptional condition; £695; exchanges, deferred terms.—John S. Truett, Ltd., 173, Westbourne Grove, W.11. Bay. 4274. (7421)

Vauxhall 14, offer—

1946-7 Vauxhall 14 saloon, fitted radio and heater, excellent condition; £725.—Fortune Green Rd., N.W.6. Ham. 2211. (7265)

1942 Vauxhall 14 saloon; £485.—Haskins, Lad-broke 1155. (7545)

38 Vauxhall 14 saloon; £290.—Kings Motors, 14, High St., Hounslow. Tel. 3532. (4906)

Vauxhall saloon, 14hp, late 1939, very clean, very attractive car; £450.—23, Florence Rd., New Cross, S.E.14. Tel. 3532. (6202)

1935 14hp saloon, black/brown, low mileage; £225.—H. A. Saunders, Ltd., 326/330, Euston Rd., N.W.1. Euston 4511. (7751)

GATEHOUSE offer 1938 Vauxhall 14hp saloon, black, fitted radio, mechanically sound; £325.—Gatehouse Motors, Ltd., Highgate Village, London, N.6. Mountview 4444. (7722)

WALTER SCOTT, Ltd.—1937 model Vauxhall 14 touring saloon, black, exceptional condition, saved Dec.; £345; terms, exchanges.—39, College Crescent, Hampstead, N.W.3 (Swiss Cottage Tube). Fri. 5914.

Vauxhall Wyvern & Velox

H. A. SAUNDERS, Ltd., offer—

1949 Vauxhall Wyvern saloon, black with fawn cloth upholstery, 8,000 miles; £895.—H. A. Saunders, Ltd., Austin House, High Rd., North Finchley, 1100 yds north of Albany Rd. Corner. Hillside 0024. (7676)

WYVERN (November, 1948) black, immaculate throughout; £825.—Campbell, Symonds, Wembley 6862. (6214)

1949 Vauxhall Velox saloon, 4,900 miles.—British & Colonial Motors, Ltd., Upper St. Martin's Lane, W.C.2. Tel. 7949. (7970)

1949 Vauxhall Velox, radio, heater, spotlights, etc., low mileage, choice of two; from £645.—Bertram Cowen, Hermitage Lane, Streatham, Fulham 2100. (7192)

1949 Vauxhall Wyvern, green, 8,000 miles only, one private owner, in first class condition; £895.—J. Hunter, Ltd., 22, Cricklewood Broadway, N.W.2. Tel. Gladstone 6303. (7970)

1948 (Nov.) Vauxhall Velox black with brown leather, low mileage, fitted radio, in immaculate condition throughout, saved; price £690.—Blue Star Garages, Ltd., 617, Finchley Rd., N.W.3. Ham. 2255. (7659)

Vauxhall Velox for sale, chauffeur driven, 1949 (August) perfect showroom condition, 2,300 miles only; £900; can be seen any time.—Apply General Manager, Watford Co-operative Society, Ltd., 138, St. Albans Rd., Watford. (7251)

Vauxhall 25

1936 Vauxhall 25 limousine, special body, very beautiful car; £200. **JACK WILLIAMS MOTORS, Ltd.**, 169, Priory Rd., Finchley, N.12. Tel. 3532. (7134)

FOR sale, 1938 Vauxhall 25hp saloon, in very good condition.—Brasell & Jones, Station Garage, Thame, Oxon. Tel. Thame 99. (7134)

DMOUDINE 1939 Doherty Enclosed, leather upholstery, exceptional throughout, black, £845. Seen.—LPE & SAUNDERS (100 Limousines: Lists posted). A Providence Court, Grosvenor Square, 2941-Mayfair.

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SHAW & KILBURN, Ltd., for Vauxhalls.

It is important that the car you purchase is in excellent condition throughout.

SELECTION of such modern Vauxhalls at

4/6 Berkeley Square, W.1. Grosvenor 4328. (0017)

BROWN'S for Vauxhall.

1948 Vauxhall 12hp saloon de luxe, small mileage; £725. (model) Vauxhall 10hp saloon de luxe, immediate condition. £685.—Brown's Garage, Loughton (Essex) 4119 (Tube). (7577)

395 gns.—1939-40 Vauxhall 12 de luxe saloon, superb condition.—Below.

135 gns.—1934 Vauxhall 14 saloon, excellent condition.—Autonips, 5, Balham High Rd., Balham 1508. (7967)

GORDON CARS (LONDON), Ltd.—1948 (Dec.) Vauxhall Wyvern saloon, 13,000 miles.—Below.

GORDON CARS (LONDON), Ltd.—1947 Vauxhall 12hp saloon, 15,000 miles.—Gordon House, 373, Euston Rd., W.1. Euston 6811. (7461)

HAMILTON MOTORS (LONDON), Ltd., 466-480, Edgware Rd., London, W.2. Paddington 0022 (12 lines) Velox, 20,000 miles, black, excellent condition throughout.

1949 14hp saloon, black, brown leather, perfect mechanical and body condition.

1940 12hp de luxe saloon, in excellent running order, body smart.

1937 12hp saloon, grey, in very good condition.

ALWAYS a good selection of used Vauxhalls in stock. A.H.M.I. will purchase for cash all Vauxhall cars, including latest models. (7776)

Vauxhall, and other makes of used cars in good condition; let us know your requirements. Tel. Oxbridge 737, Gregory's of Oxbridge. (0039)

G.R. BROTHERS (MOTORS), Ltd., main dealers 7-15, Peter St., Manchester, 2 (Bla. 9857), always have a fine selection of post-war Vauxhall models carry-over full warranty; your inspection invited. (0883)

Vauxhall Cars Wanted

C M THE CAR MART, Ltd., wish to purchase Vauxhall cars.—150, Park Lane, W.1. Grosvenor 3434. (0975)

SHAW & KILBURN, Ltd., Vauxhall main dealers

WILL purchase modern Vauxhall cars.

4/6 Berkeley Square, W.1. Grosvenor 4328. (0018)

ROWLAND SMITH'S, the Vauxhall buyers—Hamstead High St. (Hamstead Tube). Ham. 6041. (0955)

REQUIRE post-war Vauxhall urgently.—30, Ryegate Rd., S.W.16. Tube Hill 1268. (7035)

14hp saloon, black, Poolpool. Tel. 22322. (1919)

1949 Vauxhall Velox saloon required.—Turner, Ross Hill, Station Hill, Winchester. Tel. 2268. (7035)

CORBITT & TAYLOR urgently require all types of Vauxhall.—22, Conduit Mews, W.1. Amb. 0049

SIMPSON'S MOTORS (WIMBLEDY), Ltd., wish to purchase and repair Vauxhalls. Vauxhall 3985. (108)

NORTH WORCESTERSHIRE GARAGE, Vauxhall, main dealers, buy all models.—Tel. Stourbridge 5292.

BRITISH & COLONIAL MOTORS, Ltd., require used Vauxhall cars.—Upper St. Martin's Lane, W.C.1. Tel. 3598.

W no dealers.—J. Scales, 426, Chesham Road, Cambridge. (7035)

CASH buyers of low-mileage Wyvern and Velox Vauxhalls; distance no object.—Hattens, Lord St., Southampton. Tel. 2268.

MODERN low-mileage Vauxhall 10, 12 and 14hp cars urgently wanted; write full details.—Colly's Garage, Ltd., 40, Earl's Court Rd., S.W.5. Pro. 0083. (108)

HAMILTON MOTORS (LDN.), Ltd., Vauxhall main dealers, urgently require Vauxhalls of all types—466-480, Edgware Rd., London, W.2. Call, write or phone Paddington 0022. (108)

Vauxhall Spares and Service

C.A.C.

CROYDON AUTOMOBILE COMPANY, Ltd.

Vauxhall-BEDFORD rebuilt assemblies exchanged at manufacturers' repair rates, complete rebuilt Vauxhall-BEDFORD components, dynamo, starters, distributors, etc., exchanged at 50% manufacturers' list price, radiator exchange service and all other repairs to your Vauxhall or Bedford at.

TRIADWAY MOTORS CO., Ltd., 350, Tottenham Rd., N.1. Tel. Thornton Heath 3276 (14 lines). (006)

TRIAXON—Gear boxes, reconditioned units, on exchange plan, for all 10, 12, 14hp, 25hp and 30 models; stock deliveries.

TRIAXON—Suspension units, exchange or outright sale, immediate deliveries; our reconditioned units are complete with king pins and include shock absorbers overhaul, 3 months' guarantee; available for 10, 12, 14 and 25hp cars available; if in difficulty now enquiries invited.

TRIAXON—Springs, immediate delivery, suitable for 10, 12 and 14hp Vauxhalls, all models, brand new, just received from factory.

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BROADWAY MOTOR CO.

WE specialise in service replacement units for Vauxhall 10, 12 and 14hp, etc.

GEARBOXES, differential units, front suspension units, engines, large stocks of Vauxhall spares.

BROADWAY MOTOR CO., Ltd., 3-15, Russell Rd., Wembley, S.W.19. Liberty 2494/5/6. Stores: Liberty 3368. Grams Automobiles, Wembley, London. (683)

FOR Vauxhall mudguards, running boards, 1931-3, 1934-5, 1936-8, 1939-40. Quakers Rd., Brighton. (1000)

EGHAM MOTOR CO., for Vauxhall cars, spares and services.—Egham By-Pass, Egham 151. (019)

Vauxhall—Various sound parts 1921-1936, exact selection. 1927-1935.—T. H. Gibson, Westway, Witherside, Tiverton, Devon. (686)

BROMLEY & District—Consult Davis & Hill, Ltd., the local Vauxhall and Bedford spares and service specialists.—Bromley Common, Ravensbourne 2634.

Vauxhall—Fully rebuilt and guaranteed engines from £20; immediate delivery or on credit. Stirling, Carmo, Leighton Rd., N.W.5. Gul. 5555. (604)

K. J. MOTORS, Ltd.—Replacement engine and all stocks of all spares and accessories available in immediate delivery.—137/149, Widsome Rd., Bromley, Kent. Rav. 3456-7-8-9. (028)

REPAIRS! Recondition exchange suspensions (complete) pair 10, 12, 4, £15; DX. £17/10; 25hp, £25. 25hp, £30; recoloured shock absorbers.—Fri. 2247, 10, Winchester Mews, N.W.3. (1049)

VETERAN CARS

1906 Darracq 2-seater, original condition, good running order; £395.—9, Ladbroke Terrace, London, W.1. Tel. 235. (710)

WELHAM'S, Veteran Car Specialists, for sale and purchase.—5, Surbiton Hill Rd., Surbiton, Middlesex 1873. (1049)

1912 Silver Ghost Rolls-Royce fixed head coupe, believed London-Kimberly chassis, 1912, with electric and starter by Rolls-Royce in 1925 and since unmodified; the whole car including chassis and plating indistinguishable from original showroom condition, mechanically perfect and fitted with new tyres and tubes all round, as trial and Rolls-Royce inspection welcomed one of the most beautiful Edwardian cars in existence.—Apply J. H. Sarginson, Falcons Hall, Ricklinghall, Dias, Norfolk. Tel. Botesdale 85. (696)

VINTAGE CARS

SILVER GHOST Rolls-Royce chassis 1916, y.e. 1921, in running order. £50.—Apply The Duchy Garage, 176, Kensington Lane, London, S.W.11. (697)

WOLSELEY Ten tonner 1923, running daily last year, now laid up, sound and complete, space needed, offer.—Jeanette's House, Guildford Rd., Biele, (7035)

REAL enthusiast's opportunity!—6-litre Straight 4 R. Sunbeam, in excellent condition, unused since 1930 overhaul, owner posted abroad; bargain, £200; see Bromley, Kent.—Box 5943. (7037)

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COLBORNE GARAGE, Ltd., Ripley, Surrey. Tel. 2361.—Sole distributors for Great Britain. Sales service and spares. (097)

1947 Volkswagen, overhauled, new tyres, excellent condition throughout; £345.—77a, Pembury Villas, W.11. Baywater 374. (7035)

NEWNHAMS, Ltd.

1946 Wolseley 10 saloon, green with brown, low mileage, very well maintained.

NEWNHAM House, 235-7, Hammersmith Rd., London, W.6. Riverside 4646. (7035)

BEARDS of Kingston, Wolseley distributors.—Sales, spares and repairs.—102, London Rd., Kingston. Tel. 5348. (1008)

EUSTACE WATKINS, Ltd., the London Wolseley distributors and official service station, offer the finest when selling used cars.

1947 Wolseley 12 saloon, black, brown leather upholstery, 13,000 miles, one owner, in excellent condition. [7795]

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LOW mileage Wolseleys are scarce, should your model not be in stock put your names on our used cars list for early notification.

EUSTACE WATKINS, Ltd., 12, Berkeley St., W.1 (Mayfair 5951), and 12, Chelsea Manor St., S.W.3 (Fleming 8181). [7795]

TOM GARNER Ltd., offer

1949 Wolseley Four/Fifty saloon, black with brown leather, 5,000 miles. [7364]

TOM GARNER Ltd., 10-12, Peter St., Manchester, 2. Blackfriars 9265-6. [7364]

H. A. SAUNDERS, Ltd., offer—

1947 Wolseley 10 saloon, black with brown leather upholstery, 16,000 miles; £745. [7364]

H. A. SAUNDERS, Ltd., Austin House, High Rd., North Finchley (100 yds north of Tally Ho), Hildridge 0264. [7364]

£98 or nearest—1932 Wolseley Hornet coupe, taxed, insured.—Thomas, 52, Great North Rd., N.6. [7364]

1946 Wolseley 14 saloon, 14,000 miles, black with brown leather.—Johnson, Ltd., Wolseley Agents, Staines, Middlesex. Tel. 801. [7364]

£475—1940 Wolseley 10 de luxe sal., bronze, brown leather upholstery, 30,000 miles, 1 owner since original condition throughout. [7364]

MAKIN & HARRISON (MOTORS), 492-6, High Rd., Chiswick, Chiswick 0558/2619. [7685]

1947 Wolseley 18, black, 25,000 miles, new tyres, spare set remoulded, one owner, chauffeur maintained, perfect; offers over £800.—Box 6244, 17093 [7685]

WOLSELEY 25 saloon de ville, 1937, clean car, ideal for hire; gift, £275.—Frank Hendley (B'mth), Ltd., 318, Christchurch Rd., Bournemouth, Southbourne 5121. [7685]

1949 (Sept.) Wolseley 4/50, maroon, excellent condition, only 1,700 mls.; £1,195.—Eng. Smallman 21, Kenneth Crescent, N.W.2. Glis. 3055. [7110]

1948 Wolseley 18 saloon, black with brown leather, 19,000, first class throughout; £895.—Bells Service Garages, 144, London Rd., Kingston-on-Thames 1185. [7994]

£95 gns.—Wolseley 14/60, 1939 model, de luxe 4-door saloon, black, sliding head, green leather, good state, excellent condition; terms, exchanges.—Rowland Smith, Hampstead (Hampstead Tube). [7685]

1935 gns.—Wolseley Super 6 (August, 1938), 25hp fourstroke drop head coupe, grey, lawn leather, careful owner, excellent condition, taxed; terms, exchanges; list; open 9-7 week-days and Saturdays.—Rowland Smith, Hampstead (Hampstead Tube). [7685]

1938 Wolseley 12hp fixed head coupe, immaculate, engine just reconditioned; £375; terms, exchanges.—Lynch Garage (opposite G.P.O.), Uxbridge, Tel. 122. [7701]

1939 Wolseley 14hp saloon, black, exceptionally beautiful condition, absolutely spotless; £550.—Reeves Motors, Grand Parade, Forty Lane, West St., Edmonston, W.9. Tel. 4162. [7701]

£425—Wolseley 18 saloon (first registered 1938), finished in black with brown interior, immaculate car, bargain.—Grove Garage & Motors, 322, Park St., Edmonston, W.9. Tel. 4162. [7701]

WALTER SCOTT, Ltd.—1938 Wolseley 18 de luxe saloon, black, brown hide as new, a fine car; £425; terms, exchanges.—39, Colless Crescent, Hampstead, N.W.3 (Swiss Cottage Tube). Fri. 5914, 17486 [7701]

1938 (December) Wolseley 25 saloon, with division, a genuine well-kept privately owned car, almost unmarked, accept £555.—Jack Rose, Ltd., Stafford Rd., Wallington, Surrey. Wallington 6677-8. [7685]

1937 Wolseley 18hp saloon, black with brown leather upholstery, considerable sum recently spent in overhauling and repainting, taxed; £550 w near offer.—Bridge House, Brookbrooke, Herts. [7977]

1933 Wolseley limousine, 21hp, 7-seater, face for splendid hire car; £150; exchange small van, cash either way.—194, Peckham Rye, S.E.22, New Cross 4232. [7977]

1935 Wolseley 14-door saloon, black with brown leather upholstery, good tyres, in good mechanical condition, very clean motor; £185; 3 months' guarantee.—Tankard & Smith, Ltd., 97, Peckham Rd., S.E.15, Rodney 2051. [7645]

WOLSELEY 25hp limousine, 1936-7, in absolutely first class condition throughout, new engine (works) fitted, only done 3,000 miles, upholstery cloth rear and occasional, leather front (bench), partition, good tyres all round, taxed year end (hackney rate); this car offers an unusual opportunity for hire car man to acquire first class wedding car, nearest £650, trial any time, any day, any day.—Walker, 5, Millmole, Maltby, Yorks. Tel. Maltby 55. [7062]

Wolseley 4/50 Cars Wanted

CAR MART, Ltd., wish to purchase Wolseley 4 Four-Fifty, 320, Euston Rd., N.W.1, Eus. 1212. [7062]

Wolseley 6/80 Cars Wanted

CAR MART, Ltd., wish to purchase Wolseley Six-Eighties cars.—150, Park Lane, W.1. Gro. 5434. [7062]

Wolseley Cars Wanted

ROWLAND SMITH'S, the Wolseley buyers.—Hampstead High St. (Hampstead Tube). Ham. 6041. [7062]

EUSTACE WATKINS, Ltd., as sole London Wolseley distributor with the largest Wolseley clientele, are the best market for used Wolseley cars.

EUSTACE WATKINS, Ltd., 12, Berkeley St., W.1 (Mayfair 5951), and 12, Chelsea Manor St., S.W.3 (Fleming 8181). [7685]

WANTED, 1938/39 Wolseley 12hp saloon.—Vanderella, 215, Haverstock Hill, N.W.3, Prim. 4441. [7062]

WANTED, post-war Wolseley 10hp saloon.—"Relax", Victoria Place, Airdrie, Scotland. Tel. 2176. [7504]

JACK ROSE, Ltd., require low-mileage Wolseley cars.—Jack Rose, Ltd., Stafford Rd., Wallington, Surrey. Wallington 6677-8. [7504]

WEYBRIDGE AUTOMOBILES, Ltd., the Wolseley distributors, urgently require late-type Wolseley.—Tel. Weybridge 233. [0540]

C 14/60, 4/50; distance no object. H&Iona, Lord St., Southampton. Tel. 2268. [0806]

B LAKES, Wolseley distributors, will purchase any non-Covenant Wolseley car.—110, Bond St., Liverpool. Tel. 1737. [0379]

APPROACH us first before disposing of your Wolseley car.—Tankard & Smith, Ltd., 194/196, Kings Rd., S.W.3, Flaxman 4801/2/3. [6299]

1939 Wolseley 12hp series 3 seven-seater limousine.—R. Hardy & Son, 55, Marylebone High St., W.1. Tel. Welbeck 1101-3. [1764]

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W. JACOBS & SON

WE specialise in spares and repairs for all models of Wolseley cars.

W. JACOBS & SON, Mill Garage, Chigwell Rd., South Woodford, E.18. Wandstead 0680. [0485]

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FOR Wolseley mudguards, running boards, 1935-46.—Brook St., Queens Rd., Brighton. [0386]

R. HARDY & SON, 55, Marylebone High St., W.1. Tel. Welbeck 1101. Spares, reconditioned unit service and repairs for all Wolseley series models. [0571]

EUSTACE WATKINS, Ltd., Chelsea Manor St., S.W.3 (Fleming 8181), for Wolseley service; complete overhauls, coachwork and reconditioned engines. [0277]

BROOKHURST GARAGE—Harrow agents for Wolseley; sales, service, spares, reconditioned units.—Uxbridge Rd., Harrow Weald, Middlesex. Tel. Grimsdale 561. [0304]

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ambitious skilled mechanic; his pay will not be
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WORKING foreman required to take sole charge of
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—Write stating experience and wage required, Box 6206.
WORKS Foreman required for repair shop, mainly
Vauxhall, in South-West Surrey; applicants must
have experience in handling men and in turning out
first-class work; write, giving age and full details of
experience.—Box 5393. (6393)

WORKS manager wanted by well-known motor
engineers, South Kensington area; sound know-
ledge of automobile and coachwork repairs essential;
interviewer; over 40 preferred; state experience and
salary required.—Box 6347. (7014)

MOTOR fitters required by H. Beart & Co., Ltd., 102,
London, W.14, Kingston-on-Thames; must be fully
experienced, knowledge of Wolseley, Jaguar, Rover and
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GARAGE established half a century, with good agencies
in East Midlands; no living accommodation; some capital
for investment after probationary period an advantage;
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Main Dealers, 321, Romford Rd., Forest Gate E.7.
JUNIOR outside car salesman, experienced, with
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must have experience of sales in car trade and be able
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—Write, stating age, giving age and experience, and
details of last three years' employment only.—Box 6480.
(7164)

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Service Manager, Manton Motors, Ltd., 23-37, South
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MANAGER required to control motor vehicle dis-
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station; must be able to interview customers, estimate
diagnose troubles, instruct staff and test; good know-
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essential.—Write, stating age, experience and salary
required to Box 6145. (7106)

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(6244)

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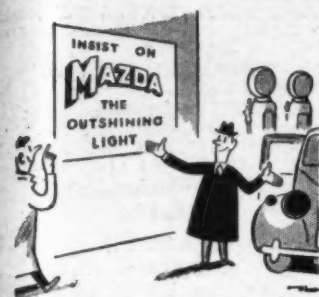
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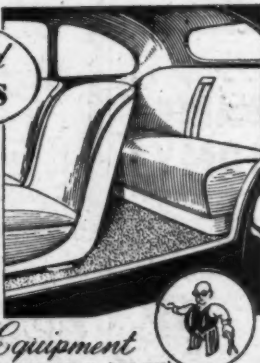
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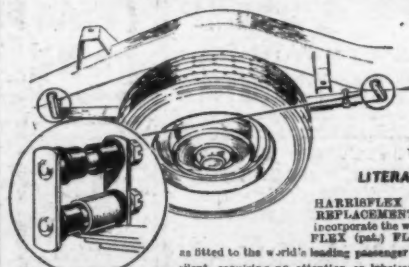
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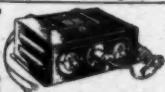
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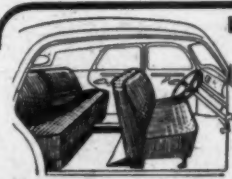
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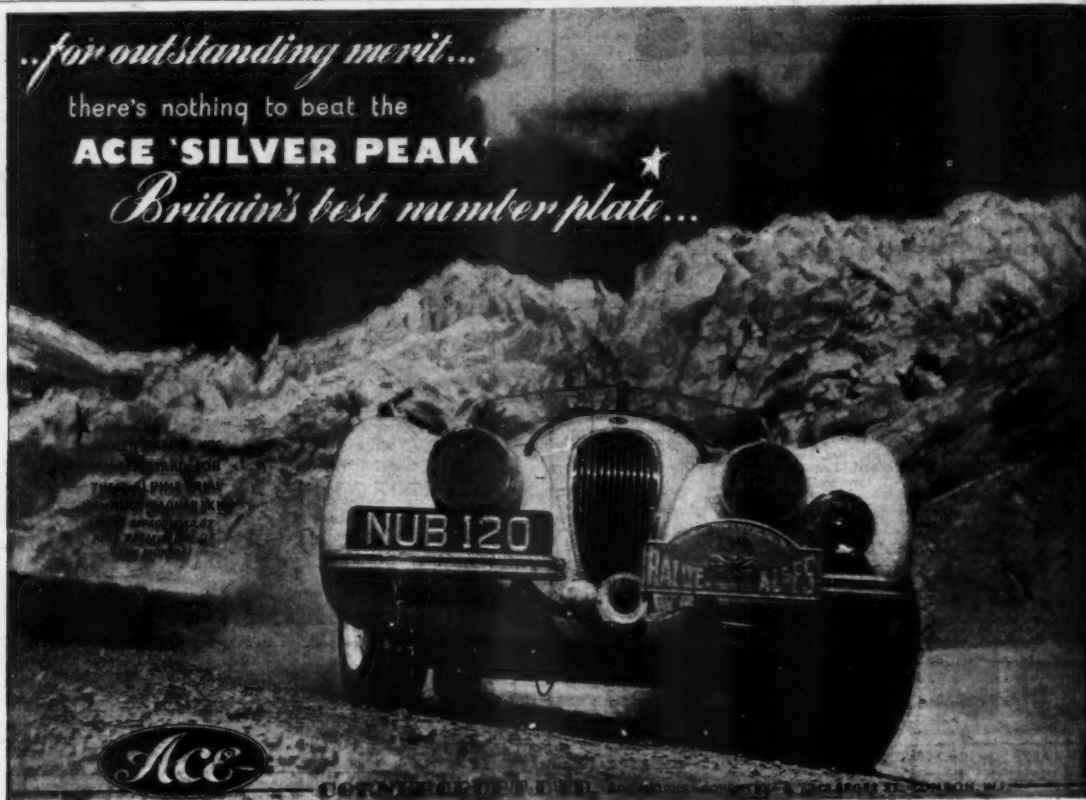
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